



# Pattern Book

CITY OF NORTH PORT  
PLANNING  
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Prepared For:  
West Villages Improvement District  
Adopted Xth 2020  
Index Map Xth 2020

**WEST VILLAGES**  
FLORIDA



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**Village District Pattern Book (VDPB)**

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# 7 Guiding Principles

The Village District Pattern Book (VDPB) is the guiding framework for Village (V) zoning within the City of North Port. The Village Land Use category and zoning district were created to enable adaptable and responsive development. All property located in the West Villages that lies within the City of North Port has this future land use and zoning category.

Current development within the West Villages relies on the VDPB and the relevant Village District Pattern Plan (VDPP). The first VDPB was approved in 2005 and was most recently updated in 2007. This Pattern Book update is a timely revision of the standards and ensures Village standards allow development that is consistent with the goals of the City of North Port Comprehensive Plan.

The VDPB lays the foundation for Village development consistent with Goal 5 of the City of North Port Comprehensive Plan. Goal 5 aims to overcome the environmental and social challenges associated with urban sprawl (please refer to The City of North Port Website for the full text). Goal 5 of the Village Land Use Classification states the following long-range goals for the West Villages:

1. Promote a sustainable pattern of development
2. Encourage a better jobs/housing balance
3. Reduce reliance on the personal automobile through sustainable development patterns
4. Protect and enhance environmental assets
5. Provide for an orderly urban to rural transition with the proper timing and location of public facilities.

By outlining responsive development standards, the intent of the VDPB is to provide a guide for both the City and developers that ensures sustainable and enduring development in the West Villages.

Utilizing a two-step Pattern Book/ Pattern Plan process ensures that the standards governing development remain timely and relevant, balancing both public and private interests.

Further, specific development standards will be identified for each Village prior to development through the VDPP process.



## 2. History and Background

The City of North Port entered into a unique partnership with the private sector in 2004 through the adoption of the Village District Pattern Planning (VDPP) process and the supporting changes to the City of North Port's land development code. This action was both new to the City of North Port and to the State of Florida. The City of North Port was the first local government to obtain authorization from (known then as) the Florida Department of Community Affairs (DCA) to locally manage a large land area in this manner.

The Pattern Book was envisioned by the City when the Village Land Use Classification was created and subsequently adopted. It is an extension of the shared vision for how to build functional communities in a more rational and effective manner. The criteria in the Pattern Book falls into the "should" and "shall" categories and is not intended to be overly regulatory or restrictive to the creativity of the private sector, or to the future needs and realities of the marketplace. It is, however, crafted to establish a broad community framework within which the private sector can express itself in the building of sustainable Villages, Neighborhoods, and Town Center.

In adopting both the Village Land Use Classification and this Pattern Book, the City of North Port examined – and in some instances altered – public policies having the power to influence community design. Of primary importance is the examination of the cost, benefit, and function of public infrastructure and how such investments might best be planned and built to support the City's vision of sustainability and inclusiveness.

The Village District Planning Process is reasonably open and flexible, yet comprehensive in its requirements for the submission and review of information needed by the City to effectively evaluate and comment on its application.

The intent of the West Villages' Village District Pattern Book is to provide a guide for both the City and the future developers of this important addition to the City of North Port. Simply stated, the Village Land Use Classification was adopted to achieve the following long-range planning vision for the City of North Port:

1. To establish the building blocks for a more sustainable pattern of development for the City of North Port.
2. To promote a better jobs/housing balance within the City of North Port.

3. To overcome the problems typically associated with urban sprawl.
4. To promote a pattern of development that will reduce reliance on the automobile.
5. To protect and enhance the environmental assets.
6. To provide for an orderly transition of land uses through a planning process that couples a build-out vision for the City of North Port with the proper timing and location of needed public facilities.
7. To adopt and utilize the Village District Pattern Planning (VDPP) process as the means for achieving this planning vision.
8. The location, size, and configuration of this annexed area has permitted the City of North Port to create this long-term planning vision for the Village Land Use Classification.

As Mr. Sam Jones, the Planning Director for the City of North Port, said in 2004 about this Pattern Book, “We look forward to partnering with the private sector to help the West Villages become a reality.”

The discussion that follows illustrates the partnership between the City and the private sector and the development that has occurred over the past several years.

The +/- 8,000-acre area of the West Villages that was annexed into the City of North Port is identified on the Index Map, Figure 3.1 on pages 10 and 11. This map further delineates the land within the West Villages that has VDPP approval, and specific development entitlements, as well as land that does not yet have VDPP approval. Updated from previous iterations, the West Villages Index map shows the extent of the West Villages less the area dedicated to State College of Florida, and the River Road Office Park located near the intersection of Playmore Road and River Road. The West Villages Boundary includes land that is owned by the Dioceses of Venice, located near the intersection of Tamiami Trail and River Road, as well as land owned by Sarasota Memorial Hospital, located at the southeast corner of West Villages Parkway and Tamiami Trail.

The vast majority of land that lies within the West Villages is under common ownership, by the West Villages Partnership. During the course of development, some areas have been sold and developed by interests other than the West Villages Partnership, these areas are detailed in the Village descriptions below.

### **Village A**

The first Village District Pattern Plan (VDPP) for Village A was adopted by the City of North Port City Commission on November 28, 2005. Neighborhoods 1 through 7 in Village A, also known as Gran Paradiso at West Villages, was purchased by Lennar Homes, and is under active development for 2,022 residential units. Neighborhood 8 in Village A is under West Villages Partnership control, where DR Horton is currently developing 200 residential units in The Preserve. On the Index Map, the northwest corner of the intersection of West Villages Parkway and Tamiami Trail identify an area that includes Town Center designation. This corner is not under the control of the West Villages Partnership. In 2007, Construction Plans for 220,000 square feet of commercial/retail development were approved on approximately 35 acres (MAS-07-126). This approved development is recognized in the Non-Residential Tracking Table, intended to be updated as development progresses.

### **Village B**

Village B is entirely under the control of the West Villages Partnership and contains the Renaissance and Oasis neighborhoods that are actively being developed with 1,200 residential units. The Village B VDPP was adopted by the City of North Port City Commission on June 8, 2015.

### **Village C**

Islandwalk, also known as Village C, was purchased by Pulte/DiVosta, and is under active development for 2,399 residential units. The VDPP for Village C was originally adopted on August 8, 2005 and amended four times. The most recent amendment was approved on January 26, 2015.

### Village D

The Village D VDPP is entirely under the control of the West Villages Partnership and includes a significant development area that is devoted to Town Center activities. The approval included 2,400 residential units, 300,000 square feet of commercial, and 200,000 square feet of office. The VDPP for Village D was approved by the City of North Port City Commission on September 26, 2017.

Consistent with current approvals for Village D, 105,000 square feet of retail/commercial space is opened in the Fall of 2019 as part of Town Center. This marketplace is comprised of a mix of local, regional, and national service providers anchored by a 45,000 square foot Publix supermarket.

### Villages E, F, & G

Villages E, F, and G have approved VDPPs dated the 24th of July 2019. Villages F and G are currently being updated subject to boundary adjustments, for consistency with buildout roadway alignments within the West Villages. Entitlements for each of these villages are reported in the Residential and Non-Residential Tracking Tables. As noted above, Village E includes land that is owned by the Dioceses of Venice, located near the intersection of Tamiami Trail and River Road, as well as land owned by Sarasota Memorial Hospital, located at the southeast corner of West Villages Parkway and Tamiami Trail.

Established in Comprehensive Plan Policy 13.1.n, portions of the West Villages that are under the control of the West Villages Partnership, are entitled for 16,400 dwelling units. As of November 2018, a total of 8,198 dwelling units have concurrency approval across the northernmost Villages, Villages A-D. The Development Tracking Table shows the distribution of these units across the West Villages.

Relying on the principles of creating sustainable development, each village contains a mix of neighborhood and village serving uses. Town Center development is proposed in appropriate locations throughout the West Villages and is intended to serve a regional market base.

The Atlanta Braves Spring Training Facility in Village G provides opportunities for the West Villages to attract visitors to the region, drawing the necessary market base for supporting Town Center development. The Atlanta Braves Spring Training Facility is approximately 117+ acres and houses the main stadium, six practice fields, auxiliary practice areas, maintenance facilities, player club facilities, a player development academy, and an event lawn.

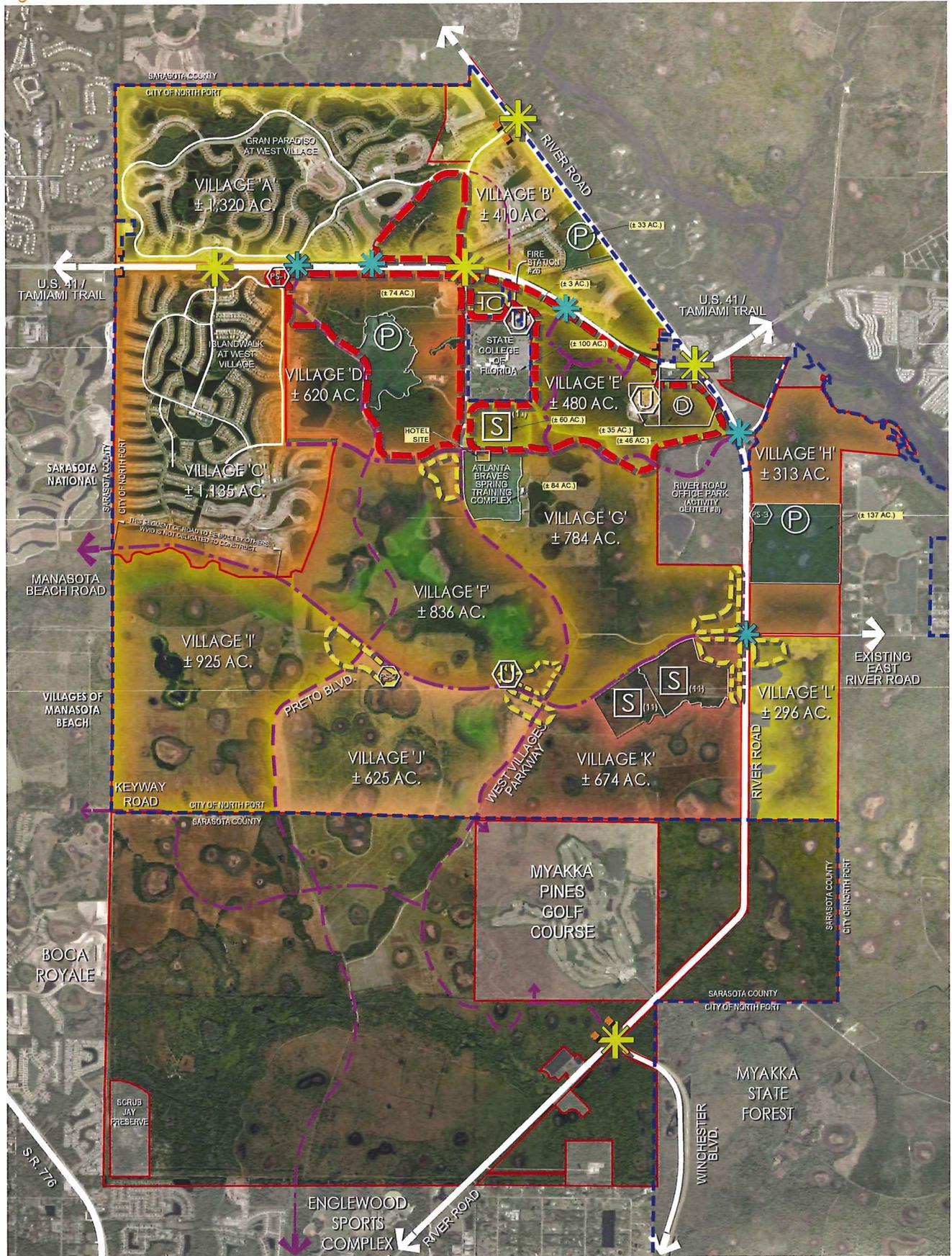


# 3. Community Context

Ensuring complete development is a key component in creating The West Villages. Continual adaptation of Village Boundaries and roadway alignments has and will continue to occur through the incremental planning of each of the Villages. This thoughtful adjustment can be seen throughout the history of the West Villages. As discussed previously, amendments to the boundaries and areas of Villages A-D have occurred through the respective Pattern Plans. With this adaptive adjustment of Village boundaries, the responsive adjustment of Town Center boundaries is necessary to reinforce a sustainable network within, and between each of the Villages. When identifying commercial and retail areas within the West Villages, ensuring the compact critical mass exists to sustain business is essential for the vibrancy of both residential and commercial development. Based on market trends, and reduced demand for office and retail space, the Village Centers have been adjusted and located in areas that will encourage each Village to have a sustainable mix of residential, commercial, and office uses to serve West Villages residents and visitors. Ensuring that neighborhood areas continue to be sustainably served, adjustments have also been made to Town Center boundaries. The land area has increased from previous approvals, so Town Center will be the focal point for region-serving commercial and retail development.

The Village Index Map, Figure 3.1, provided on the following page, illustrates the land area and layout of the West Villages.

Figure 3.1



# West Villages Index Map

# WEST VILLAGES INDEX MAP

## LEGEND

|   |   |   |   |   |   |   |   |
|---|---|---|---|---|---|---|---|
|  | EXISTING ROADWAY                          |  | APPROXIMATE SCHOOL LOCATION (K-12) (11)                     |  | RECREATIONAL / OPEN SPACE / RECREATIONAL TRAIL SYSTEM |  | APPROXIMATE TOWN CENTER LOCATION (2)                            |
|  | 4 LANE W/ NEV. & 8' SIDEWALKS             |  | SARASOTA MEMORIAL HOSPITAL - HEALTH CARE SERVICES ± 28 A.C. |  | POTENTIAL ROADWAY CONNECTION TO ADJACENT COMMUNITY    |  | EXISTING TRAFFIC SIGNAL   |
|  | 2 LANE W/ NEV. 8' SIDEWALK, & 5' SIDEWALK |  | APPROXIMATE UTILITIES / PUBLIC FACILITIES LOCATION (10)     |  | STATE COLLEGE OF FLORIDA                              |  | APPROXIMATE FUTURE FULL MEDIAN OPENINGS; TRAFFIC CONTROL T.B.D. |
|  | 2 LANE W/ NEV. & 5' SIDEWALKS             |  | APPROXIMATE PARK LOCATION (10)                              |  | APPROXIMATE VILLAGE CENTER LOCATION (2)               |  | CITY OF NORTH PORT - CITY LIMITS                                |
|  | POTENTIAL WEST VILLAGES GATEWAY LOCATIONS |  | DIOCESE OF VENICE LOCATION                                  |  | FIRE/ POLICE STATION LOCATIONS                        |   |   |

## NOTES

- (1) ACREAGES ARE APPROXIMATE. FINAL ACREAGE LOCATIONS SUBJECT TO REFINEMENT BASED UPON EXISTING SITE CONDITIONS AND FINAL VILLAGE DISTRICT PATTERN PLANS.
- (2) AREAS DEFINED WITHIN THIS DESIGNATION WILL INCLUDE 300-1,000 ACRES OF TOWN CENTER AND MAY INCLUDE VILLAGE DEVELOPMENT AT THE DENSITY / INTENSITY ESTABLISHED IN THE V.D.P.B., CONSISTENT WITH SECTION 53-212 U.L.D.C. FINAL LOCATIONS WILL BE DETERMINED DURING THE V.D.P.P. PROCESS.
- (3) THE INDEX MAP DEPICTS THE GREENBELT FRAMEWORK BY IDENTIFYING VILLAGE EDGES ADJACENT TO MAJOR ENVIRONMENTAL FEATURES OR ROADWAYS AND MULTI-USE PATHWAYS. ADDITIONALLY, THIS FRAMEWORK CONNECTS VILLAGE CENTERS AND TOWN CENTERS, CONSISTENT WITH COMPREHENSIVE PLAN POLICY 13.8 AND SEC. 53-216 OF THE U.L.D.C. COMPLIANCE WITH THESE POLICIES WILL BE FURTHER DEMONSTRATED WITH FUTURE VILLAGE DISTRICT PATTERN PLANS.
- (4) LOCATION AND CONFIGURATION OF PARKS SHALL BE DETERMINED BY AGREEMENT BETWEEN THE PROPERTY OWNER AND THE CITY OF NORTH PORT, IN ACCORDANCE WITH THE PRINCIPLES OF AGREEMENT.
- (5) MULTI-MODAL TRAILS MAY BE SATISFIED BY AN 8' SIDEWALK AND 4' BIKE LANES, AND SHALL BE FURTHER REFINED WITH FUTURE VILLAGE DISTRICT PATTERN PLANS.
- (6) 8' - 12' TRAILS WILL BE DELINEATED ON V.D.P.P., AND MAY BE COMBINED WITH INTERNAL ROADWAYS.
- (7) THE WEST VILLAGES IMPROVEMENT DISTRICT WILL COORDINATE WITH THE APPLICABLE GOVERNING AUTHORITY TO ESTABLISH ACCESS MANAGEMENT CRITERIA.
- (8) FINAL LOCATIONS OF TOWN CENTERS, PUBLIC FACILITY SITES, SCHOOL SITES, VILLAGE BOUNDARIES AND ROADWAY CORRIDORS SHALL BE DETERMINED DURING THE V.D.P.P. PROCESS AND/OR DEVELOPERS AGREEMENT.
- (9) FINAL ALIGNMENT OF CORRIDORS / ROADWAYS IS SUBJECT TO FINAL APPROVAL OF JURISDICTIONAL AGENCIES.
- (10) FINAL SIZE, LOCATION, AND CONFIGURATION OF UTILITIES/PUBLIC FACILITIES SHALL BE DETERMINED BY AGREEMENT BETWEEN THE PROPERTY OWNER AND THE CITY OF NORTH PORT.
- (11) SCHOOL SITE SHOWN IN VILLAGE E WILL BE REMOVED IN FAVOR OF SCHOOL SITES SHOWN IN VILLAGE K UPON APPROVAL OF AN AGREEMENT WITH SARASOTA COUNTY SCHOOL BOARD.

Approved XXX XX, 2020  
ORDINANCE NUMBER 2020-XX  
PETITION NUMBER VIA-XX-XXX

March 5, 2020





# 4. Regulatory Framework

**“Goal 1: Ensure that the character and location of land uses maximize the potential for economic benefit and the enjoyment of natural and man-made resources by citizens while minimizing the threat to health, safety and welfare posed by hazards, nuisances, incompatible land uses, and environmental degradation.**

**Objective 1: Future development activities shall continue to be directed in appropriate areas as depicted on the Future Land Use Map and shall encourage the use of innovative land development regulations, consistent with sound planning principles, minimal natural limitations, the goals, objectives and policies contained within this plan, and the community character.”<sup>1</sup>**

This Village District Pattern Book is intended to establish a community framework for West Villages that encourages innovative design and sound planning principles in an area of the City that has been identified as an appropriate location for growth and development. This Pattern Book is further intended to enable development interests the flexibility to express themselves through the development of Villages without restrictive regulations that hamper creativity or adaptability to changing market conditions. Areas with approved Pattern Plans shall proceed through the development process under the relevant Village District Pattern Plan (specifically portions of Villages A-D (as shown on Exhibit 3.1)). All other land that lies within the West Villages Improvement District, that has not obtained VDPP approval shall rely upon this Pattern Book during the process of development. Village District Pattern Plans for Villages E, F, and G will be consistent with and governed by this Pattern Book upon its adoption. Consistent with 53-219 (A) of the City of North Port ULDC, deviations may be requested by the applicant, but shall be specified in the VDPP and approved by the City.

<sup>1</sup> City of North Port Comprehensive Plan Goal 1, Objective 1

To further these objectives, the West Villages Review Committee (WVRC) was created and authorized by the City Commission in 2017. This committee includes the following representatives:

- One individual who represents the Master Developer
- Two individuals appointed by the West Villages Improvement District

The WVRC is responsible to review architectural features, design components, and landscape plans for retail/commercial, Mixed-Use, and residential buildings and sites within the West Villages.

The WVRC will rely upon the Design Standards contained in this VDPB, those established for each individual village, contained in the VDPPs, as well as the ULDC, for guidance in making approval decisions.

Granting authority to the WVRC does not eliminate the City's review authority to evaluate applications to confirm compliance with this VDPB, the respective VDPP, and the ULDC.

Unless otherwise indicated, specifications relating to landscaping contained in the City of North Port Unified Land Development Code are the minimum standard the Design Review Committee is empowered to approve. Upon approval of each VDPP, future application submittals to the City of North Port must include evidence of approval from the WVRC prior to approval of an applicable development order by the City of North Port.

In furtherance of the interpretation authority granted by the City of North Port Comprehensive Plan and Unified Land Development Code (ULDC), the Zoning Administrator/Planning Manager shall have the authority to administratively approve modifications initiated by the Master Developer, of standards and the conceptual design layout contained within VDPPs. The Zoning Administrator/Planning Manager may impose reasonable mitigation measures to limit impacts from the requested adjustment of standards. The Zoning Administrator/Planning Manager shall have the added authority to administratively approve modifications to standards initiated by the Master Developer that provide a benefit to the general public or surrounding community, as long as those modifications do not increase density, maximum allowable building heights, reduce buffer widths along existing and future public thoroughfares, or add permitted uses.

This community framework, established for West Villages, encourages an innovative approach that relies on sound planning principles. The adaptable regulatory framework will encourage sustainable land development.





# 5. Environmental Planning & Protection



*Figure 5.1*

**Open space pond**

The Environmental Standards section provides an opportunity to further the environmental planning and protection goals defined by the City of North Port, while establishing a framework for the holistic environmental protection of the natural assets of the West Villages. These standards are in accordance with all applicable Federal, State, and Local Standards.

Required in the Comprehensive Plan, measures such as greenbelts serve a key role enhancing the sustainability and environmental performance of the West Villages.

Functioning as environmental corridors greenbelts may provide the additional direct benefits of:

- environmental systems linkages
- water resource enhancements
- recreational spaces

The inclusive design of these corridors provides further environmental benefits of land conservation through reduced sprawl and reduction in vehicle miles travelled through the promotion of alternate transit. Consistent with Policy 13.8 of the Comprehensive Plan Transfer of Development Rights shall be allowed up to two (2) dwelling units per acre in exchange for the provision of Greenbelts. This provision allows focused density and residential clustering, accompanied with the provision of neighborhood serving uses that are proximate to users of those services.

Encouraging flexibility and creativity in Village design is essential to ensuring sustainability and habitat integrity in the West Villages. This flexibility is vital to upholding the foundational goals of environmental protection and preservation. To ensure the development of ecologically sustainable villages, a context sensitive approach must be taken when conceptualizing village design. To reach this goal, a balance must be achieved between the goals of development density and intensity and habitat preservation. The West Villages are primarily comprised of upland pasture that is predominantly agricultural land, and native habitats (predominantly wetland). In order to maintain their ecological and silviculture viability, wetlands of significant size should be connected to greenbelt and other open space systems. Rather than enclosing each village or neighborhood with a greenbelt, greenbelts should focus on preserving environmentally sensitive land while connecting neighborhoods and villages through a multimodal network.

The integration of greenbelts as buffers between uses should be carefully considered in order to maintain habitat integrity. In places where adjoining uses are not conducive to providing greenbelts as features for environmental protection other forms of buffering such as tree plantings or foliage should be considered. Focusing systems of environmental protection on specific areas of high-quality habitat enhances the integrity of the system as a whole; over the preservation of larger areas of marginal habitat.

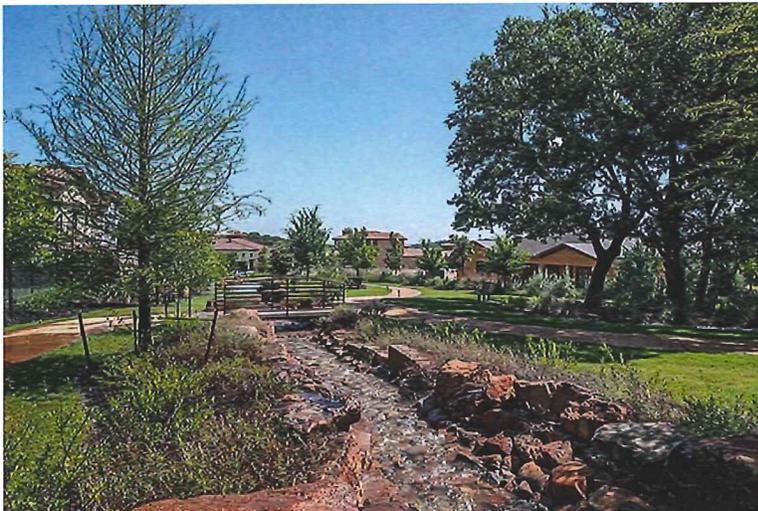


Figure 5.2

**Neighborhood Pathways**

Consistent with the Comprehensive Plan Policy 13.8 and ULDC Sec. 53-216 village greenbelts for West Villages are comprised of several elements that will be shown on all future VDPPs and will take on more definition during site design and development. The village greenbelt may include perimeter buffers and/or perimeter walls, preserved environmental features, wetlands and wetland buffers, as well as adjacent roadways and multi-use pathways within rights-of-way. These elements are intended to discourage sprawl by providing a definable village edge.



# 6. Roadway & Pathway Standards

Roadways and pathways within the West Villages will comfortably accommodate vehicular, pedestrian, Neighborhood Electric Vehicles, and bicycle traffic. Through design and development of each Village, the proposed maximum posted speed will be up to 35 miles per hour, to encourage the use of Neighborhood Electric Vehicles as an alternative mode of transportation for trips within the West Villages. A sidewalk system will be constructed to facilitate pedestrian circulation. In addition, roadways will be landscaped and lit to enhance the community appearance and contribute to pedestrian comfort.

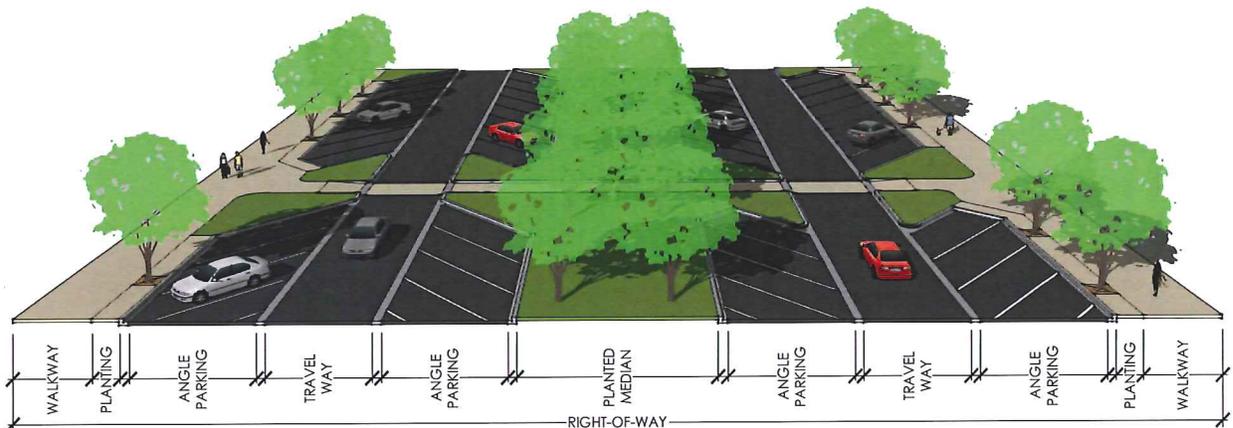
The following cross-sections reflect design standards for each type of street that is represented on the Index Map. Variations may occur based on location, application, and neighborhood/village design; however, the general scale and components should be maintained. The Roadways and Pathways depict improvements that are impact fee and mobility fee creditable and are to be constructed and maintained by the WVID and/or the Master Developer.

## Main Street

Main Street is a slow movement local roadway providing a pedestrian environment and on-street parking for the commercial main street.

- Slow movement local roadway that offers a pedestrian environment for the commercial Main Street.
- Typically a two-lane divided roadway with angled or parallel parking on either side of the travel lane.
- May be designed for 25 mile per hour speed limit.
- May provide for public frontages with plantings containing trees.
- May provide mid-block un-signalized walkways to promote pedestrian connectivity.
- Designed to accommodate pedestrian, bicycle and Neighborhood Electric Vehicles.

| Design Parameters                          | Min.         | Max. |
|--|--------------|------|
| Target Speed                               | 15           | 25   |
| Travel Way Dimensions                      | Min.         | Max. |
| Travel Lane(s) Width (ft)                  | 12           | 17   |
| Parking Lane Width (Parallel Parking) (ft) | 8            | 9    |
| Parking Lane Width (Angled Parking) (ft)   | 18           | 21   |
| Public Frontage                            | Min.         | Max. |
| Planting Type                              | Intermittent |      |
| Planting Width (ft)                        | 0            | 6    |
| Walkway Width (ft)                         | 10           | -    |
| Right Of Way                               | Min.         | Max. |
| ROW Width                                  | 120          | 200  |

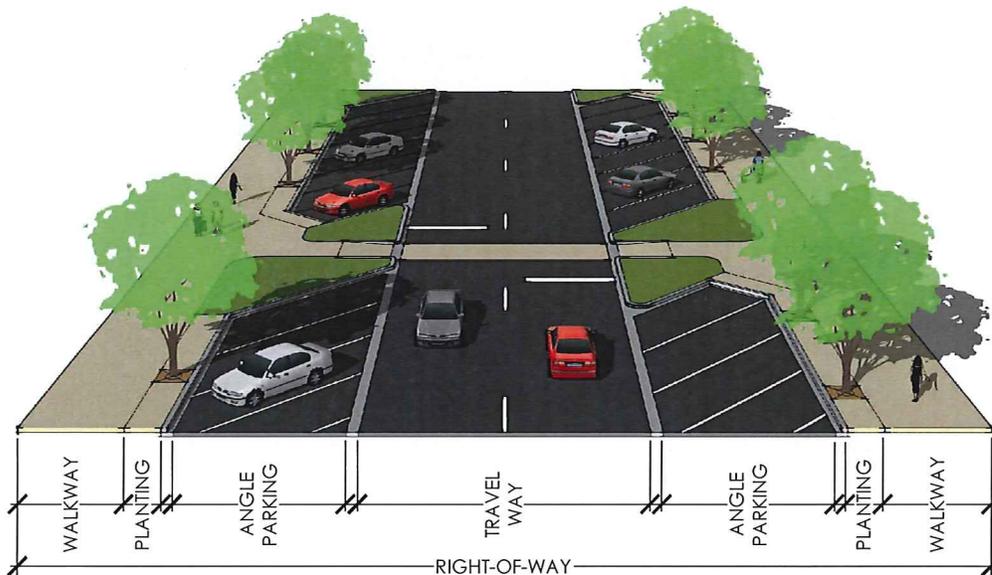


### Main Street Alternative

Main Street Alternative is a slow movement local roadway providing a pedestrian environment.

- Slow movement local roadway.
- Typically, a two-lane undivided roadway.
- May be designed for 25 mile per hour speed limit.
- May provide for public frontages with plantings containing trees.
- May provide mid-block un-signalized walkways to promote pedestrian connectivity.
- Designed to accommodate pedestrian, bicycle and Neighborhood Electric Vehicles.

| Design Parameters                          | Min.         | Max. |
|--|--------------|------|
| Target Speed                               | 15           | 25   |
| Travel Way Dimensions                      | Min.         | Max. |
| Travel Lane(s) Width (ft)                  | 12           | 17   |
| Parking Lane Width (Parallel Parking) (ft) | 8            | 9    |
| Parking Lane Width (Angled Parking) (ft)   | 18           | 21   |
| Public Frontage                            | Min.         | Max. |
| Planting Type                              | Intermittent |      |
| Planting Width (ft)                        | 0            | 6    |
| Walkway Width (ft)                         | 10           | -    |
| Right Of Way                               | Min.         | Max. |
| ROW Width                                  | 120          | 200  |

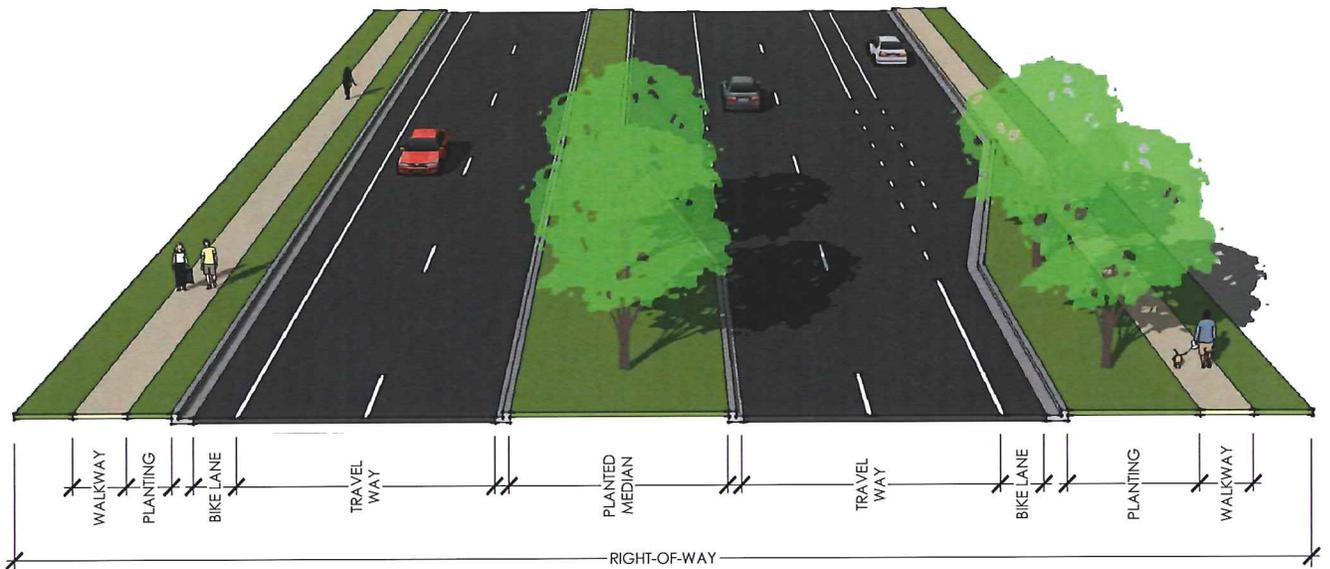


### Boulevard Option 1

Boulevards are generally two- to four- lane divided thoroughfares.

- Typically, a two- to four lane divided thoroughfare
- May be designed for up to 35-mile per hour speed limit.
- May include trees and landscaping in median and public frontages.
- Designed to accommodate pedestrian, bicycle, and small electric-powered vehicles.

| Design Parameters                          | Min.          | Max.         |
|--|---------------|--------------|
| Target Speed                               | 15            | 35           |
| Travel Way Dimensions                      | Min.          | Max.         |
| Travel Lane(s) Width (ft)                  | 11            | 14           |
| Turn Lane Width                            | 10            | 12           |
| Bike Lane Width (ft)                       | 5             | 7 (Buffered) |
| Parking Lane Width (Parallel Parking) (ft) | N/A           | N/A          |
| Parking Lane Width (Angled Parking) (ft)   | N/A           | N/A          |
| Public Frontage                            | Min.          | Max.         |
| Planting Type                              | Planted/Grass |              |
| Planting Width (ft)                        | 0             | -            |
| Walkway Width (each side) (ft)             | 5             | -            |
| Right Of Way                               | Min.          | Max.         |
| ROW Width                                  | 100           | -            |

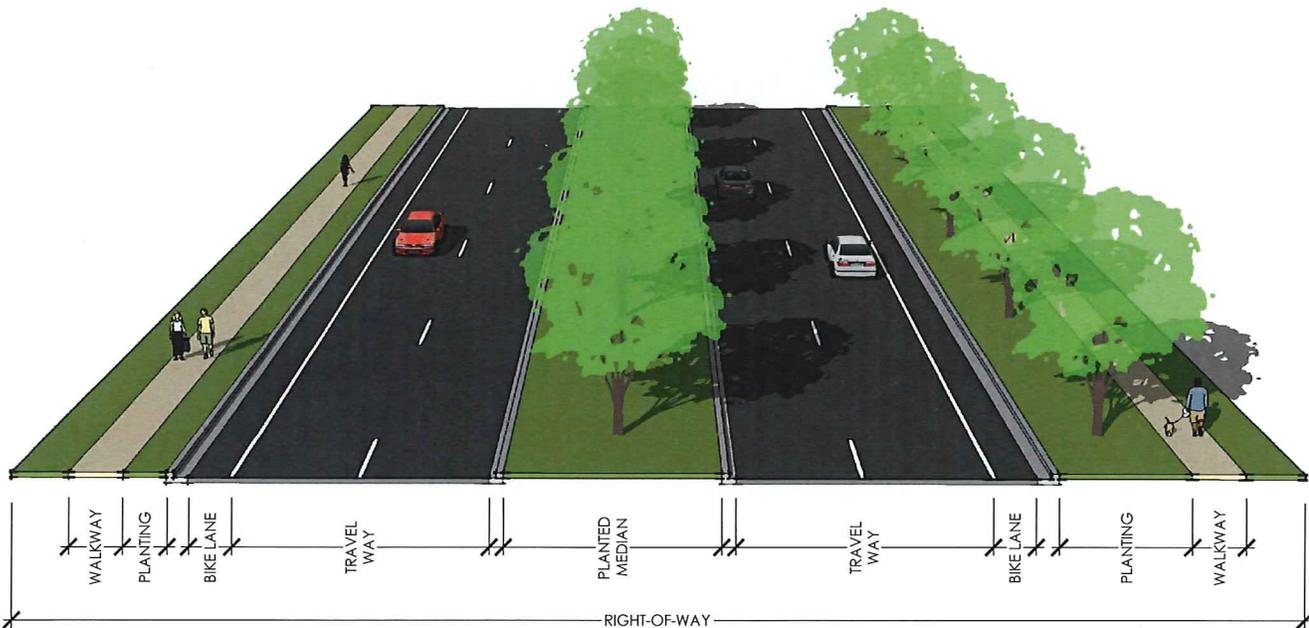


## Boulevard Option 2

Boulevards are generally two- to four-lane divided thoroughfares.

- Typically, a two- to four lane divided thoroughfare
- May be designed for up to 35-mile per hour speed limit.
- May include trees and landscaping in median and public frontages.
- Designed to accommodate pedestrian, bicycle, and small electric-powered vehicles.

| Design Parameters                          | Min.          | Max.         |
|--|---------------|--------------|
| Target Speed                               | 15            | 35           |
| Travel Way Dimensions                      | Min.          | Max.         |
| Travel Lane(s) Width (ft)                  | 11            | 14           |
| Turn Lane Width                            | 10            | 12           |
| Bike Lane Width (ft)                       | 5             | 7 (Buffered) |
| Parking Lane Width (Parallel Parking) (ft) | N/A           | N/A          |
| Parking Lane Width (Angled Parking) (ft)   | N/A           | N/A          |
| Public Frontage                            | Min.          | Max.         |
| Planting Type                              | Planted/Grass |              |
| Planting Width (ft)                        | 0             | -            |
| Walkway Width (each side) (ft)             | 5             | -            |
| Right Of Way                               | Min.          | Max.         |
| ROW Width                                  | 100           | -            |

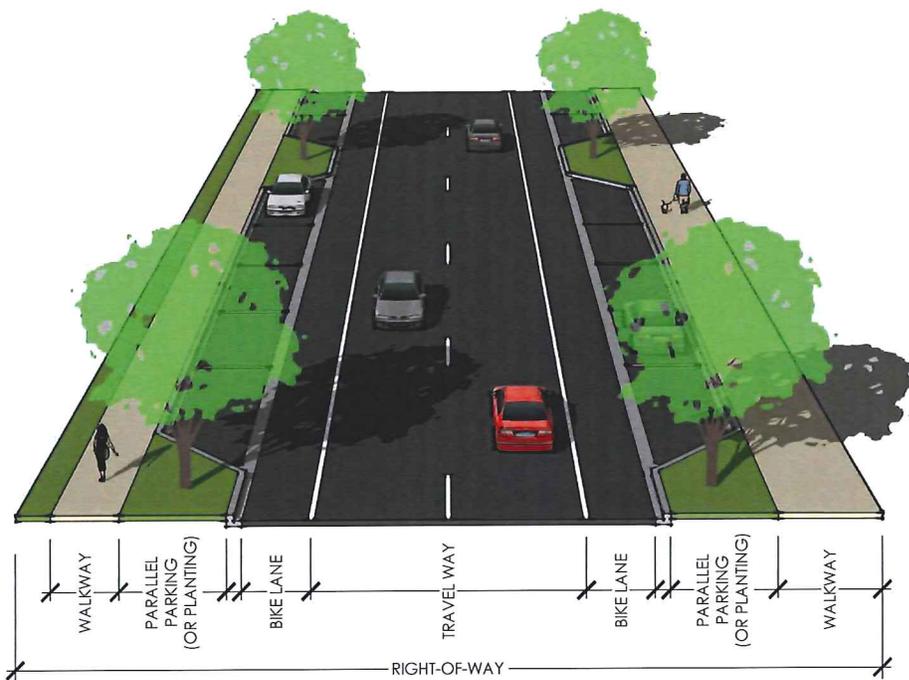


### Mixed-Use Street A

Mixed-Use Streets are generally free movement local roadways that provide on-street parking for higher density areas, including shops, offices, multi-family buildings, and townhouses.

- Free movement thoroughfare as it enters a full access intersection.
- Typically, a two- to four-lane divided roadway with turn lanes.
- Provides regional connections to the other villages and arterials.
- May be designed for up to 35 mile per hour speed limit.
- May include trees and landscaping in median and public frontages.
- Designed to accommodate pedestrian, bicycle, and Neighborhood Electric Vehicles.

| Design Parameters                                   | Min.         | Max.         |
|---|--------------|--------------|
| Target Speed  | 15           | 25           |
| Travel Way Dimensions                               | Min.         | Max.         |
| Travel Lane(s) Width (ft)                           | 11           | 12           |
| Bike Lane Width (ft)                                | 5            | 7 (Buffered) |
| Optional Parking Lane Width (Parallel Parking) (ft) | 8            | 9            |
| Parking Lane Width (Angled Parking) (ft)            | N/A          | N/A          |
| Public Frontage                                     | Min.         | Max.         |
| Planting Type                                       | Intermittent |              |
| Planting Width (ft)                                 | -            | 6            |
| Walkway Width (ft)                                  | 5            | -            |
| Right Of Way  | Min.         | Max.         |
| ROW Width   | 60           | 90           |

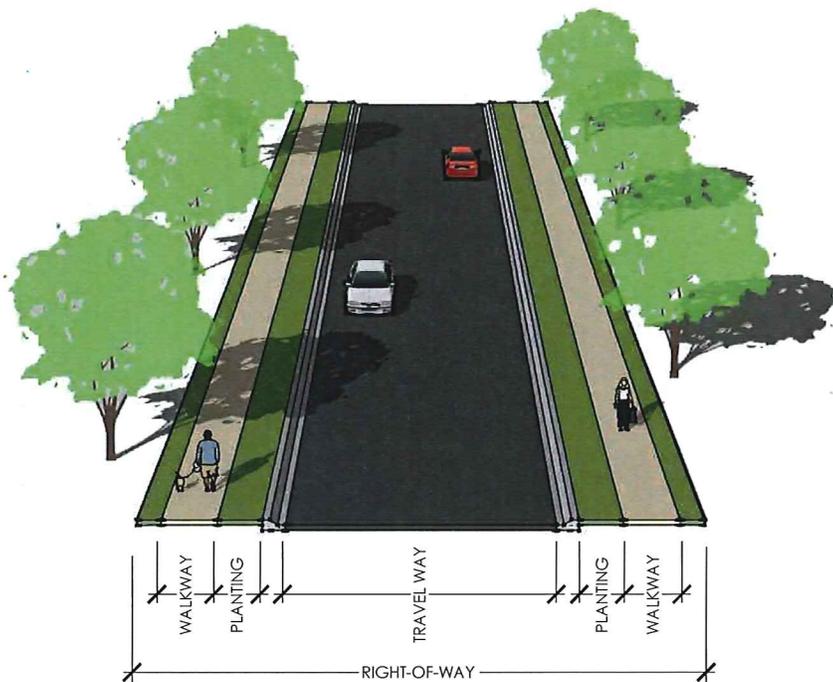


### Mixed-Use Street B

Mixed-Use Streets are generally free movement local roadways that provide on-street parking for higher density areas, including shops, offices, multi-family buildings, and townhouses.

- Free movement local roadway.
- May provide frontage and on-street parking for higher density uses including shops, offices, multi-family buildings, and townhouses.
- Typically, a two-lane undivided roadway.
- May accommodate parallel parking.
- May be designed for 25-mile per hour speed limit.
- Designed to accommodate pedestrian connectivity with sidewalks, separated from vehicular traffic with landscaping, as well as bicycles and Neighborhood Electric Vehicles.

| Design Parameters                                   | Min.         | Max.        |
|---|--------------|-------------|
| Target Speed  | 15           | 25          |
| <b>Travel Way Dimensions</b>                        | <b>Min.</b>  | <b>Max.</b> |
| Travel Lane(s) Width (ft)                           | 11           | 12          |
| Bike Lane Width (ft)                                | N/A          | N/A         |
| Optional Parking Lane Width (Parallel Parking) (ft) | 8            | 9           |
| Optional Parking Lane Width (Angled Parking) (ft)   | N/A          | N/A         |
| <b>Public Frontage</b>                              | <b>Min.</b>  | <b>Max.</b> |
| Planting Type                                       | Intermittent |             |
| Planting Width (ft)                                 | 4            | -           |
| Walkway Width (ft)                                  | 5            | -           |
| <b>Right Of Way</b>                                 | <b>Min.</b>  | <b>Max.</b> |
| ROW Width   | 54           | 80          |

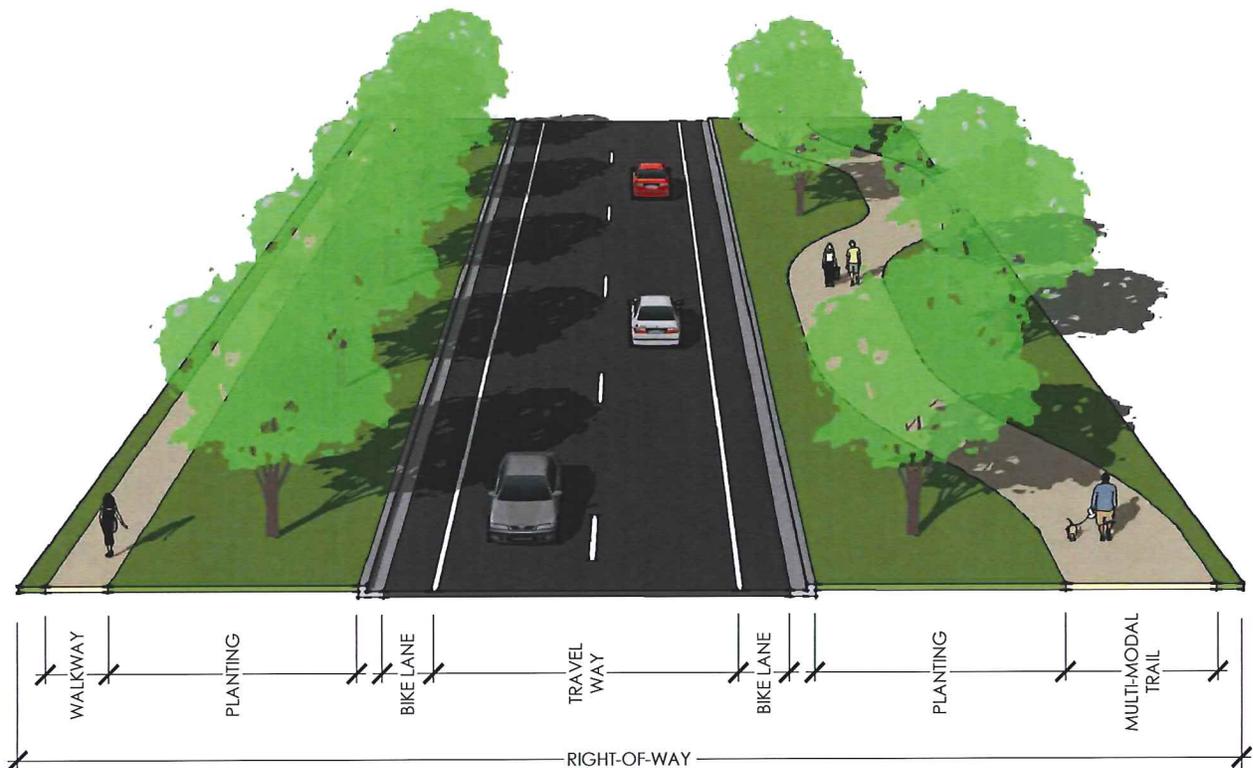


## Avenues

Avenues are frequently used roadways, intended to connect neighborhoods to one another. They are typically free movement local roadways that provide circulation and connectivity to other areas of the community. Avenues are intended to divert higher traffic volumes away from residential neighborhoods while allowing for interconnectivity within the Village.

- Frequently used roadways, intended to connect neighborhoods to one another.
- Typically, a two-lane undivided roadway.
- Designed to accommodate pedestrian, bicycle, and Neighborhood Electric Vehicles (may be a trail).
- May be designed for 35-mile per hour speed limit.
- May be lined with canopy trees.

|  |               |              |
|--|---------------|--------------|
| <b>Design Parameters</b>                   | <b>Min.</b>   | <b>Max.</b>  |
| Target Speed                               | 25            | 35           |
| <b>Travel Way Dimensions</b>               | <b>Min.</b>   | <b>Max.</b>  |
| Travel Lane(s) Width (ft)                  | 11            | 12           |
| Optional Bike Lane Width (ft)              | 5             | 7 (Buffered) |
| Parking Lane Width (Parallel Parking) (ft) | N/A           | N/A          |
| Parking Lane Width (Angled Parking) (ft)   | N/A           | N/A          |
| <b>Public Frontage</b>                     | <b>Min.</b>   | <b>Max.</b>  |
| Planting Type                              | Planted/Grass |              |
| Planting Width (ft)                        | 4             | -            |
| Optional Walkway Width (ft)                | 5             | -            |
| Multi-Modal Trail Width (ft)               | 8             | 12           |
| <b>Right Of Way</b>                        | <b>Min.</b>   | <b>Max.</b>  |
| ROW Width                                  | 50            | 100          |

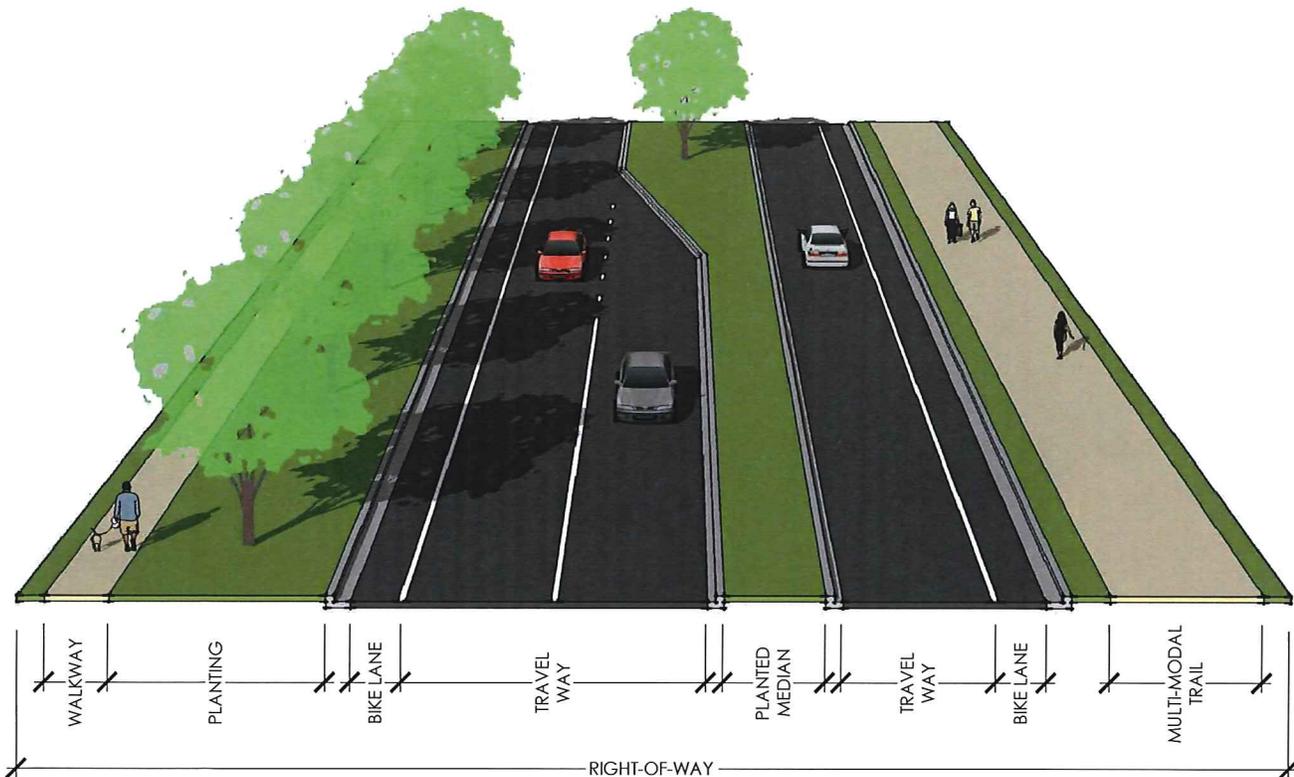


## Avenue at Intersection

Frequently used roadways, intended to connect neighborhoods to one another.

- Typically a two-lane undivided roadway.
- Designed to accommodate pedestrian, bicycle and Neighborhood Electric Vehicles (may be a trail).
- May be designed for 35-mile per hour speed limit.
- May be lined with canopy trees.
- Turn lanes are included to accommodate safe turning movements.

| Design Parameters                          | Min.        | Max.         |
|--|-------------|--------------|
| Target Speed                               | 25          | 35           |
| <b>Travel Way Dimensions</b>               | <b>Min.</b> | <b>Max.</b>  |
| Travel Lane(s) Width (ft)                  | 11          | 12           |
| Turn Lane Width (ft)                       | 10          | 12           |
| Optional Bike Lane Width (ft)              | 5           | 7 (buffered) |
| Parking Lane Width (Parallel Parking) (ft) | N/A         | N/A          |
| Parking Lane Width (Angled Parking) (ft)   | N/A         | N/A          |
| <b>Public Frontage</b>                     | <b>Min.</b> | <b>Max.</b>  |
| Planting Type                              | Planted     |              |
| Planting Width (ft)                        | 4           | -            |
| Optional Walkway Width (ft)                | 5           | -            |
| Multi-Modal Trail Width (ft)               | 8           | 12           |
| <b>Right Of Way</b>                        | <b>Min.</b> | <b>Max.</b>  |
| ROW Width                                  | 60          | 120          |

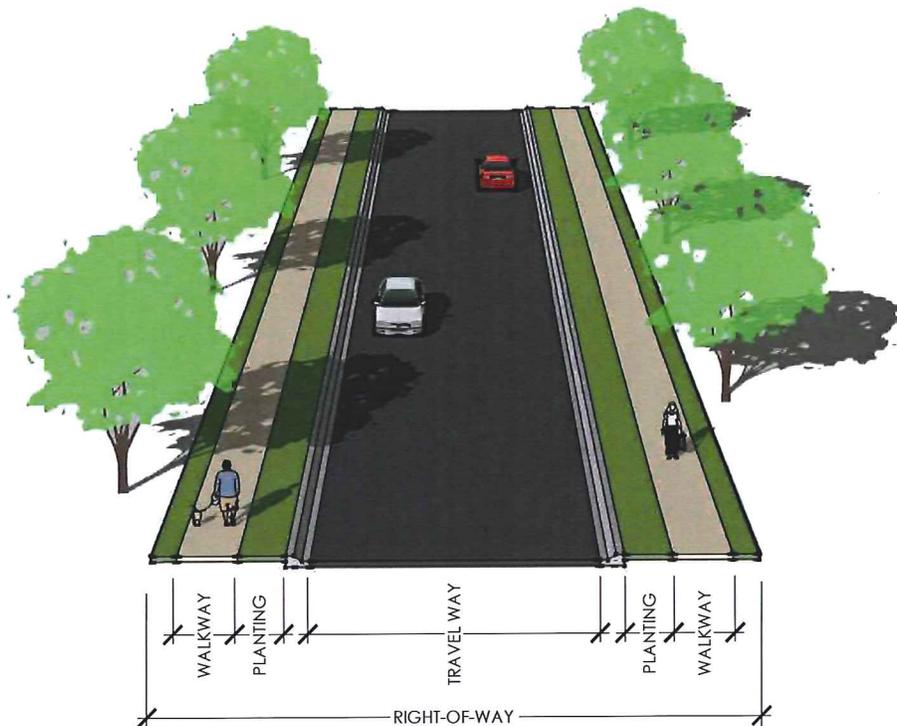


### Residential Neighborhood Street

Residential Neighborhood Streets are roadways intended for use in residential neighborhoods. They are intended to accommodate neighborhood vehicles, pedestrians, bicycles, and electric vehicles. They are typically two-lane, undivided roads intended to link Neighborhoods and Neighborhood Centers to Avenues.

- Small scale, slow movement local roadways suitable for neighborhoods.
- Typically two lanes, undivided.
- Link neighborhoods and Neighborhood Centers to Avenues.
- May be designed for speed limits of up to 25-miles per hour, typically posted lower.
- Accommodates neighborhood vehicles, pedestrians, bicycles and Neighborhood Electric Vehicles.

| Design Parameters                          | Min.          | Max.        |
|--|---------------|-------------|
| Target Speed                               | 15            | 25          |
| <b>Travel Way Dimensions</b>               | <b>Min.</b>   | <b>Max.</b> |
| Travel Lane(s) Width (ft)                  | 10            | 12          |
| Turn Lane Width (ft)                       | N/A           | N/A         |
| Bike Lane Width (ft)                       | N/A           | N/A         |
| Parking Lane Width (Parallel Parking) (ft) | N/A           | N/A         |
| Parking Lane Width (Angled Parking) (ft)   | N/A           | N/A         |
| <b>Public Frontage</b>                     | <b>Min.</b>   | <b>Max.</b> |
| Planting Type                              | Planted/Grass |             |
| Planting Width (ft)                        | 4             | -           |
| Walkway Width (ft)                         | 5             | -           |
| <b>Right Of Way</b>                        | <b>Min.</b>   | <b>Max.</b> |
| ROW Width                                  | 40            | 60          |



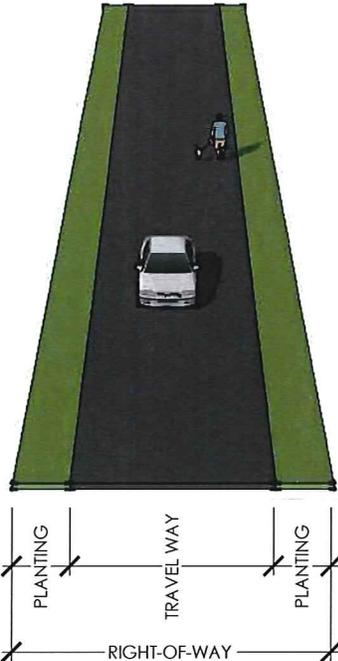
NOTE: Sidewalk only required along frontage of residential units.

### Residential Neighborhood Alley

Alleys are small-scale, slow movement drives for rear access to homes.

- Provide access to rear loading garages or parking areas.
- Designed with 10-foot travel lanes.
- Designed as one-way sections.
- Intended for “traditional neighborhood designs” in neighborhoods.
- Intended to provide access to rear of commercial/Mixed-Use buildings in Mixed-Use Areas and Mixed-Use Residential Neighborhood.

| Design Parameters                          | Min.        | Max.        |
|--|-------------|-------------|
| Target Speed                               | N/A         | N/A         |
| <b>Travel Way Dimensions</b>               | <b>Min.</b> | <b>Max.</b> |
| Travel Lane(s) Width (ft)                  | 10          | 16          |
| Turn Lane Width (ft)                       | N/A         | N/A         |
| Bike Lane Width (ft)                       | N/A         | N/A         |
| Parking Lane Width (Parallel Parking) (ft) | N/A         | N/A         |
| Parking Lane Width (Angled Parking) (ft)   | N/A         | N/A         |
| <b>Public Frontage</b>                     | <b>Min.</b> | <b>Max.</b> |
| Planting Type                              | N/A         | N/A         |
| Planting Width (ft)                        | N/A         | N/A         |
| Walkway Width (ft)                         | N/A         | N/A         |
| <b>Right Of Way</b>                        | <b>Min.</b> | <b>Max.</b> |
| ROW Width                                  | 15          | 25          |





# 7. Neighborhood Standards

The objectives of Neighborhoods include:

- Encourage housing diversity supportive of a mix of economic levels and age groups
- Encourage a sense of community by incorporating, through neighborhood design, a Neighborhood Center, which may include a civic space to accommodate a park, elementary school or other neighborhood serving civic facility
- To further a sense of community, neighborhoods should generally be located within half a mile of a Neighborhood Center or and 1-2 miles of a Village Center
- Encourage neighborhood resiliency by hardening facilities, such as civic structures, schools, clubhouses and other public/quasi-public structures, where appropriate, so those structures may provide shelter for hurricanes or other natural hazards and offer a safe environment for residents and employees.

The following conceptual site plan graphics offer a general example of neighborhood site design.

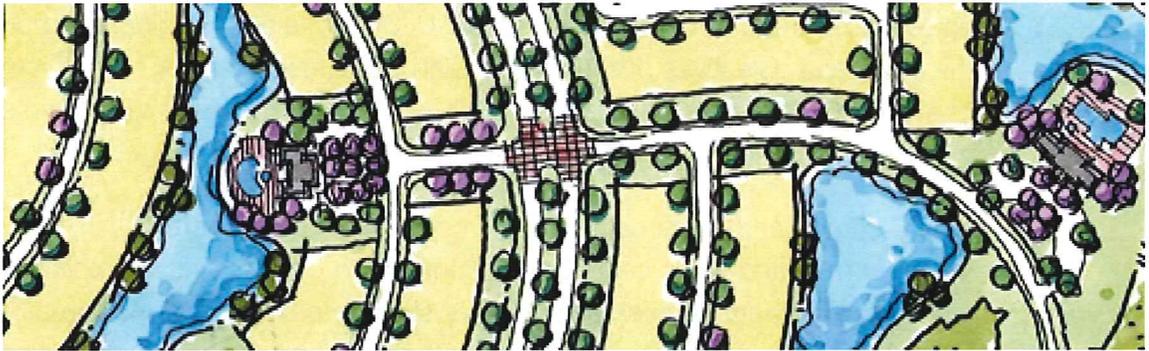


Figure 6.1

## Neighborhood/Residential Concept

Buildings, landscape, and hardscape areas are intended to provide a comfortable pedestrian and vehicular environment and may include outside gathering spaces such as squares, plazas, and parks. Architectural styles will be complementary and create visual interest within neighborhoods. A coordinated vehicular and pedestrian network will allow for comfortable and functional pedestrian and vehicular movement.

Consistent with the Comprehensive Plan and ULDC, neighborhood size should generally not exceed five hundred (500) adjusted gross acres, unless modified during the VDPP process. Neighborhood facilities, and civic services, such as passive and/or active parks should be encouraged within neighborhoods. Some other elements that should be encouraged, with design of neighborhoods are as follows:

1. Encourage a variety of housing types
2. Attached dwellings are encouraged for property near a neighborhood center
3. Civic space should be provided in each neighborhood, where appropriate
4. Each neighborhood should be designed so residences are within a one-half mile radius of a neighborhood center, where appropriate.

Density and intensity shall be calculated for land areas identified on each individual VDPP (Mixed-Use, Mixed-Use Residential Neighborhood, Residential Neighborhood, Institutional/Commercial/Mixed-Use, etc.).

### **Residential Uses**

The range of uses permitted within neighborhoods is intended to support the goals and objectives of the Comprehensive Plan and ULDC, while encouraging the implementation of the goals and objectives of the Village Land Use Category and Zoning District. The range of permitted uses shall be established for each village with each VDPP and should include those uses permitted by the City of North Port and the West Villages.

Model homes and sales centers shall be permitted anywhere within the West Villages. Model homes and sales centers may continue to operate until all residences have been initially sold. Model homes/sales centers shall be permitted to include all functions that may be associated with residential sales transactions. Model homes/sales centers may be constructed prior to final certification of all infrastructure in a phase, with approval of the Building Official. Up to ten (10) model homes and twenty (20) parking spaces may be constructed in each neighborhood, upon approval of an Infrastructure Plan, identification of a water source, and provision of stabilized access. Parking spaces and model homes shall be landscaped in accordance with this Pattern Book and the VDPP for an individual village. Upon completion of sales, parking spaces should be removed.

Utility services such as ground-mounted transformers, wells, storage tanks and lift stations shall be allowed anywhere within a Village.

## Residential Site Development

Re-platting, subdividing, and establishing required utility easements will be subject to the review and approval, and the requirements of the governing agencies, utility companies, etc.

Design should be conscious of providing an open space network of pedestrian walkways, open areas, and buffers throughout the site. This open space network should be designed to promote and provide pedestrian/bicycle access from the public and private streets to neighborhoods and individual buildings.

Connections and transitions to and from adjacent and nearby properties shall be provided through the use of the multimodal transportation network, as well as appropriately located plazas, public spaces, and landscape materials throughout the site.

Areas on site that are not devoted to structures, pedestrian areas, or paved vehicle use areas, should be either mulched or landscaped, as determined by the WVRC, and, where applicable, irrigated. A continuous concrete curb shall protect all landscaped areas adjacent to roads and parking areas unless Low Impact Development methods are used.

No outside antennas, antenna poles, antenna masts, antenna towers, satellite dish, or electronic devices shall be permitted unless screened from view, or as approved by the WVRC.

Temporary construction facilities and signs are permitted during construction. This includes construction signs, screening, and fencing. All such temporary facilities and signs shall be further subject to applicable City Codes and Ordinances. If enclosures are used for screening purposes, walls, or fences, up to a height of eight-feet (8') with opaque screening gates shall be used.

Garbage and trash containers or compactors, oil tanks, bottles, gas/LP tanks, tank exchanges and irrigation system pumps may be underground, or placed in screened areas, with landscaping. If enclosures are used for screening purposes, walls, or fences, up to a height of eight-feet (8') and finished opaque screening gates shall be used.

All air-conditioning units, mechanical equipment, etc., shall be located to minimize offsite noise impacts, and contained or shielded to minimize visibility, as determined by the WVRC. The equipment shall also comply with applicable manufacturer installation and maintenance standards. All ground mounted air-conditioning units shall be screened with a minimum 3' hedge.

The use of decorative paving is permitted. Suggested finishes and products include colored concrete, concrete pavers, stamped concrete, brick pavers or natural stone suitable for outdoor/pedestrian application.

Site clearing shall be permitted upon approval of appropriate development order for:

- i. Plat
- ii. Minor site and development plan.
- iii. Major site and development plan.
- iv. Subdivision plan, infrastructure areas only.
- v. Special exception approval.





# 8. Town and Village Center Provisions

Town Center and Village Centers within the West Villages should be designed as diverse spaces for a well-integrated mix of uses. Town Center and Village Centers have established minimum and maximum land area requirements, as defined in the corresponding section. In order to ensure consistency of treatment across these sections the following standards shall be consistent for both Town Center and Village Centers.

Land area dedicated to vertically integrated mixed-use buildings and associated parking, shall count toward commercial/office/ residential for the purpose of calculating Land Use Mix, based on use at time of establishment. Should first phase ground floor residential convert to non-residential, the land use mix allocation shall be reevaluated. Any residential unit that is developed within a mixed-use building shall be counted towards the overall entitlement for the West Villages.

To ensure incremental development of Town Center and Village Centers, consistent with the density and intensity thresholds established with each individual VDPP, and the required Land Use Mix, a Tracking Chart, prepared by the Master Developer, shall be included on the cover sheet of each Site & Development Plan, Subdivision/Infrastructure Plan, Plat/Replat, and/or Major Site and Development Plan.

Public, Non-Profit, and Institutional uses are permitted and shall count toward non-residential and/or residential intensity and/or density for the purpose of calculating land use mix. The analysis of impacts for those uses is the responsibility of the public, non-profit or institutional entity (i.e. approved commercial entitlement may not be utilized for public/nonprofit/institutional uses unless otherwise approved by the Master Developer).



# 9. Town Center Standards

The objectives for Town Center development include:

- Achieve harmony of development with neighboring buildings and areas of development
- Achieve a better environment by employing superior design and planning standards

Town Center is intended to have a functional and attractive development pattern that creates a network of successful urban style nodes. These nodes bring together commercial, residential, mixed-use, and civic spaces in a walkable destination where people can live, work, shop, and play. These Town Center nodes demonstrate the commitment to creating a multi-layered public realm, consistent with the intent of Policy 13.2 of the Comprehensive Plan and Section 53-212 of the ULDC. These Town Center nodes are intended to create a prosperous and varied network of uses supported by the residents while remaining accessible to a larger regional market base. Market trends illustrate the importance of promoting adaptable residentially supported commercial nodes within destinations that serve a broader market base than the immediate locale.

## Town Center Design Principles

| Land Use Mix   | Minimum Land area Required | Maximum land area required |
|--|----------------------------|----------------------------|
| 1. Residential   | 10%                        | 60%                        |
| 2. Commercial and Retail Services                      | 30%                        | 85%                        |
| 3. Office  | 0%                         | 30%                        |
| 4. Light Industrial                                    | 0%                         | 20%                        |
| 5. Overall Commercial, Office and Industrial (2, 3, 4) | 30%                        | 85%                        |
| 6. Parks and Open Space                                | 10%                        | No Max                     |



*Figure 9.1*  
**Example  
of Public/  
Commercial  
Space**



*Figure 9.2*  
**Town Center  
Vignette**

To understand the relationship between non-residential areas and the surrounding neighborhoods, the land use mix shall be calculated for the total acreage of all Town Center areas across all villages. Calculating uses across all Town Center nodes ensures appropriate clustering of uses, while allowing for a prosperous and varied mix of non-residential uses across The West Villages.

Density and intensity shall be calculated for land areas identified on each individual VDPP (Mixed-Use, Mixed-Use Residential Neighborhood, Residential Neighborhood, Institutional/Commercial/Mixed-Use, etc.).

The following conceptual site plan graphics offer conceptual examples of encouraged flow and movement in Town Center areas.

Buildings, landscape, and hardscape areas are intended to provide a comfortable pedestrian and vehicular environment and may include outside gathering spaces such as squares, plazas, and parks. Architectural styles will be complementary and create interest within Town Center areas. A coordinated vehicular and pedestrian network will allow for comfortable and functional pedestrian and vehicular movement.



# 10. Village Center Standards

The following represent the goals of Village Center development:

- Provide a mix of thoughtful and diverse uses in compact form
- Offer a focal point for activities to serve neighborhoods in each Village

Village Centers should be designed to provide compact traditional style neighborhood development. While containing a mix of uses, Village Center development should be focused around the daily needs of residents within each Village. Consistent with the City of North Port Comprehensive Plan Policy 13.5.2 and the type of development promoted in Village Centers, these areas shall be a maximum of 50 adjusted gross acres.

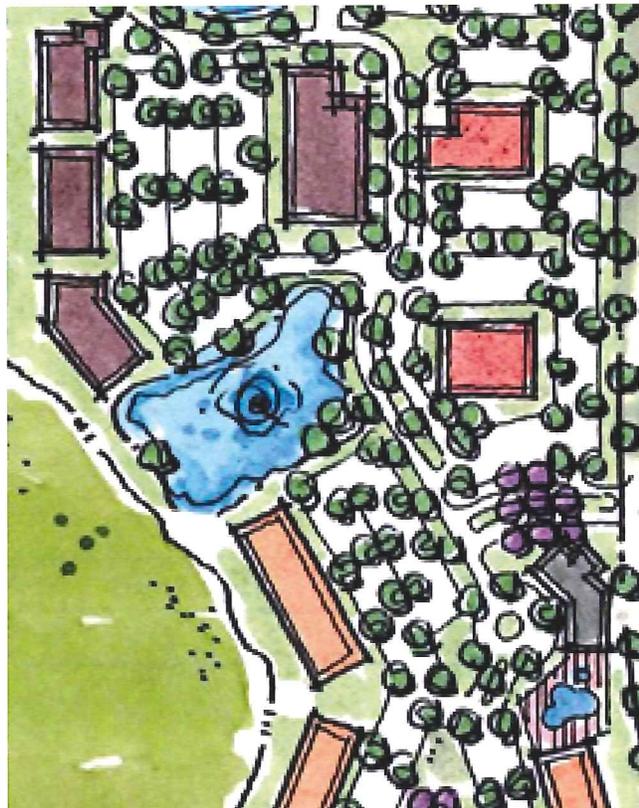
## Village Center Design Principles

| Land Use Mix                         | Minimum Land area Required | Maximum land area required |
|--------------------------------------|----------------------------|----------------------------|
| 1. Residential                       | 25%                        | 40%                        |
| 2. Commercial and Retail Services    | 20%                        | 60%                        |
| 3. Office                            | 10%                        | 25%                        |
| 4. Overall Business (2 & 3 combined) | 30%                        | 60%                        |
| 5. Civic                             | 5%                         | No Max                     |
| 6. Parks and Open Space              | 10%                        | No Max                     |

Reinforcing the goals of the West Villages, Village Centers should be designed to work in conjunction with the neighborhoods which surround them. When designing Village Centers significant consideration should be given to the needs of the surrounding neighborhoods while ensuring that the residential critical mass is present in order to support and maintain thriving commercial, office, and retail areas within the West Villages.

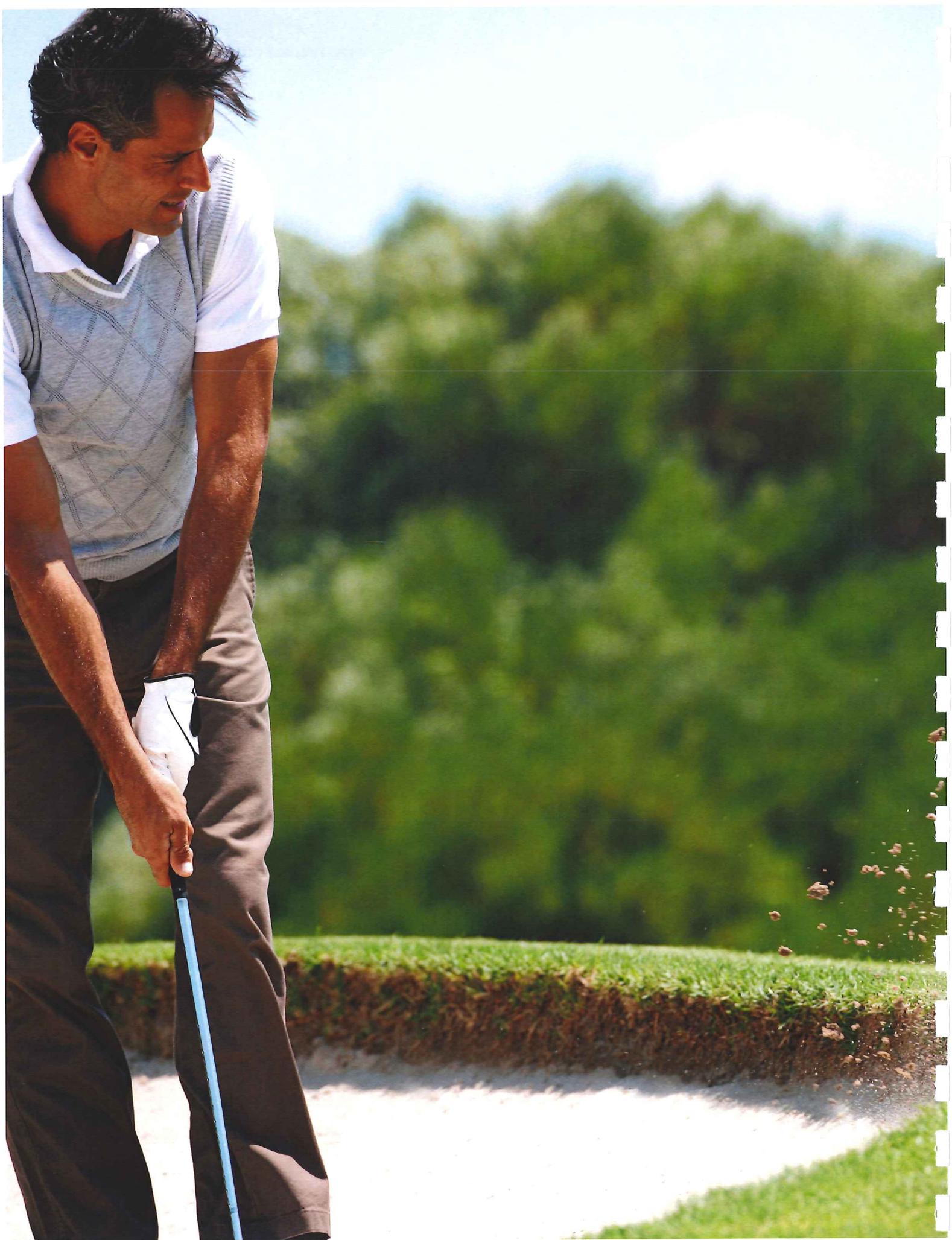
In areas where proximate location of Village Centers can be shown to improve resident service, while reducing vehicle trips, Village Centers may be located less than half a mile from each other.

The following conceptual site plan graphic offers a conceptual example of a Village Center.



*Figure 10.1*  
**Example of  
Village Center**





# 1.1 Non-Residential Uses

The range of permitted uses shall be established for each village with each VDPP and shall include those uses permitted by the City of North Port and the West Villages. Other accessory uses are permitted. Additionally, utility services such as ground-mounted transformers, wells, storage tanks and lift stations shall be allowed anywhere within the Village.

The following uses shall be permitted in Town Center areas of West Villages, and shall not require a Special Exception.

1. Private clubs and lodges.
2. Bars, cocktail lounges, nightclubs and taverns for on-premise consumption of alcoholic beverages. A bar, cocktail lounge, nightclub or tavern, established for the sale of alcoholic beverages or on-site consumption of alcohol shall be located at least five hundred (500) feet away from an established church or elementary, middle, or secondary school. Such distance shall be measured by taking the shortest route of ordinary pedestrian travel along public thoroughfare from the main entrance of the vendor's place of business to the main entrance of the nearest church or nearest edge of school property.  
  
Bars, cocktail lounges, nightclubs and taverns shall be permitted in mixed use building. Residences above or adjacent to such establishments shall be permitted.
3. Alcoholic beverage standalone retail stores provided distances specified in (2) above are followed.
4. Restaurants which derive at least 51 percent of the gross revenues from the sale of food and nonalcoholic beverages. No minimum distance is required from places of worship or schools.

The following uses are not permitted:

1. There shall be no permanent outside display or storage of materials, products, or goods except in designated areas approved by the WVRC. Events such as "sidewalk sales" are permitted on a periodic basis. There shall be no advertising flags, pennants, streamers, or the like displayed on any building or on the property unless they are a part of the overall project identity graphics and approved by the WVRC.

2. Free-standing “kiosk” type buildings or small light structures are permitted in the general areas or service areas outside of the main building.
3. No commercial vehicle of any kind shall be permitted to be parked on the property for a period of more than four (4) hours unless said vehicles are temporarily present, necessary, and incidental to the business on the property. No commercial vehicle shall be parked overnight, except within a screened enclosure (out of public view).
4. Overnight parking of recreational vehicles and boats, boat trailers or trailers of any kind, and campers or mobile homes may be permitted to park on or near common areas (i.e. parking lots) within Town Center, as authorized by the WVRC.
5. None of the vehicles named in 3. and 4. above shall be used as a domicile or residence, either permanent or temporary .
6. Utility trailer display and storage shall not be allowed.
7. Adult book stores or other “adult” oriented businesses or establishments shall not be allowed.

### **Non-Residential Site Development**

Re-platting, subdividing, and establishing required utility easements will be subject to the review and approval, and the requirements of the governing agencies, utility companies, etc.

Design should be conscious of providing an open space network of pedestrian walkways, open areas, and buffers throughout the site. This open space network should be designed to promote and provide pedestrian/bicycle access from the public and private streets to individual buildings.

Connections and transitions to and from adjacent and nearby properties shall be provided through the use of the multimodal transportation network, as well as appropriately located plazas, public spaces, and landscape materials both around the structure and throughout the site.

Areas on site that are not devoted to structures, pedestrian areas, or paved vehicle use areas, must be covered with an acceptable material, as determined by the WVRC (i.e. mulched or landscaped), and, where applicable, irrigated per the minimum standards in the Landscape and Irrigation Standards contained in this section. A continuous concrete curb shall protect all landscaped areas adjacent to roads and parking areas unless Low Impact Development methods are used.

No outside antennas, antenna poles, antenna masts, antenna towers, satellite dish, or electronic devices shall be permitted unless screened from view, unless otherwise permitted by state or federal law, or as approved by the WVRC. Freestanding communication tower sites may be approved by the WVRC and shall be subject to the design and building standards of the City, as identified in the ULDC.

Temporary construction facilities and signs are permitted during construction. All such temporary facilities and signs shall be reviewed and approved by the WVRC and subject to applicable City Codes and Ordinances.

Garbage and trash containers or compactors, oil tanks, bottles, gas/LP tanks, tank exchanges and irrigation system pumps may be underground, or placed in screened areas, with landscaping. If enclosures are used for screening purposes, walls, or fences, up to a height of eight-feet (8') and finished opaque screening gates shall be used.

All air-conditioning units, mechanical equipment, etc., whether roof mounted or ground mounted shall be located to minimize offsite noise impacts, and contained or shielded to minimize visibility, as determined by the WVRC. The equipment shall also comply with applicable manufacturer installation and maintenance standards. All ground mounted air-conditioning units shall be screened with a minimum 3' hedge.

Provisions may be made through site design, to provide adequate space for service and delivery vehicles to park at the rear of buildings, where physically possible. For buildings that are within 10 feet of an interior roadway that provides on-street parking (i.e. Mixed-Use street), the available on-street parking may be utilized for service and delivery vehicles where no other alternative exists.

The use of decorative paving is permitted within the project. Suggested finishes and products include colored concrete, concrete pavers, stamped concrete, brick pavers or natural stone suitable for outdoor/pedestrian application.

Site clearing shall be permitted upon approval of appropriate development order for:

- i. Plat
- ii. Minor site and development plan.
- iii. Major site and development plan.
- iv. Subdivision plan, infrastructure areas only.
- v. Special exception approval.

# 12. Public Buildings & Architectural Styles

Following general Village Principles (Policy 13.1.e.) public and civic buildings are an important element of the community fabric. These buildings and spaces must be thoughtfully integrated into the design of Town Center.

Utilizing distinct and identifiable elements for public and civic buildings plays a vital role in reinforcing a community's identity and culture. The façade of public buildings should clearly articulate its role in the community. Civic buildings should foster a sense of civic pride that is both welcoming and inclusive to a wide range of socio-economic and age groups, consistent with Policy 13.1.c of the City of North Port Comprehensive Plan.

Public and civic buildings should be configured to define the space as a destination, providing visually engaging spaces for public interaction. Public buildings should comprise clearly articulated entrance features that help to guide and move the public through the space. Addressing the way that public and civic buildings interface with the outdoor public spaces is key when creating a multilayered public realm. Where appropriate, the use of functional architecture is encouraged. The use of thoughtful furnishings in the public realm is also encouraged to create flexible spaces and further signify the importance of the space within the community.



Figure 11.1

**Example of Public Building**

Public and civic buildings shall have a consistent architectural style across all villages. Relying on constant and recognizable architectural elements provides welcoming and familiar civic spaces. Architectural styles used for public and civic buildings should be consistent with the design guidelines provided in this document; additionally, regionally appropriate architecture should be used to create authentic public spaces. This interface between public building and open public space should encourage connectivity to the greater community via a multi-modal network.

To ensure the consistent and context sensitive development of the West Villages, all public building projects will be reviewed by the West Villages Review Committee (WVRC).



# 13. Design Standards

The architectural fabric for West Villages is intended to provide a unique community experience that draws its architectural context from the local identity. The elements in the overall architectural arrangement are designed to encourage individuality. To that end, multimodal trails, pedestrian paths and an integrated roadway network should connect Neighborhoods and Town Centers throughout the West Villages. The integrated multimodal network, as well as the built environment should help to establish a sense of community within the West Villages. These regulations are intended to promote unique design that furthers the broader goals of West Villages. For that reason, each individual developer is encouraged in their design efforts to creatively develop a community atmosphere with canopy overhangs, transoms and signage personalities that will reflect individual product presentation, while retaining the integrity of the West Villages.

No prerequisites or predetermined design solutions shall be enforced through the VDPP process. Each design shall be evaluated on its individual merit and creativity. It is not the intent to, in any way, constrain the expression of personality.

Notwithstanding the above, by the control of both the quality of materials and range of color scheme choices, a unique community atmosphere which both enhances the individual neighborhoods, shops, and businesses, and establishes a cohesive setting, will be created. To that end, the Master Developer will establish and maintain design standards, and an architectural review process, for residential and commercial buildings.

## Signage Standards

The following signage standards are intended to augment the City of North Port's signage regulations, while establishing guidance for signage within West Villages. Signage is an important element but should not dominate the landscape. For that reason, the following standards are intended to guide the size, style, type, placement, and architectural elements of signage within the West Villages.

Except for traffic control signage in conformance with the requirements of regulatory agencies and Manual on Uniform Traffic Control Devices (MUTCD), and public purpose signage on West Villages Improvement District Property and rights-of-ways; no signs, either permanent or temporary, shall be erected or displayed on the property, or any building, structure, or window, unless the placement, character, form, size, and time of placement of such sign comply with the standards or comparable standards approved by the West Villages Review Committee (WVRC). In case of conflicts, these requirements shall apply. The West Villages Improvement District shall issue approvals for any proposed signs on its roads or rights-of-way.

Building wall signs, logos, and insignia will receive review of the following elements:

1. size
2. style
3. types
4. placement

National logos, flags, or insignias will be allowed, provided they are sized, executed, and placed, out of the sight triangles.

Building sign design shall be submitted in accordance with the Sign Criteria. Signs shall be either internally illuminated or externally illuminated. No exposed fluorescent exterior illumination shall be allowed.

The WVRC shall review and approve all logos, flags, and insignia. They shall determine color palate consistency and compatibility of the architectural finishes for signs, to ensure overall theme, as well as compatibility between buildings and signage. Sign types and associated permitted square footage of sign face are permitted for each lot, parcel, or building. Each lot, parcel or building shall be entitled to erect each sign type.

### Pylon Signs

A free-standing sign permanently affixed to the ground by a support of at least thirty percent (30%) of the sign itself.

**Area:** Pursuant to ULDC Section 29-12. D.(a). and (b), Pylon signs shall be permitted a seventy-five (75) square feet for a building up to one hundred fifty (150) feet in length. For buildings over one hundred fifty (150) linear feet, the sign area shall be 0.50 square feet of sign area per linear foot of building frontage.

Maximum sign area for Pylon signs shall be one hundred twenty (120) square feet.

### Monument/Community Entrance Signs

A sign constructed on the ground with a continuous footing or foundation with the base at grade, this may be for residential or non-residential development.

Monument/Community Entrance signs may include tenant or development name, dependent on location.

**Area:** Maximum of 100 square feet per sign face.

**Location:** 1 monument sign (maximum height of 10 feet) per outparcel in Mixed-Use Areas.

Entry features for residential development shall be allowed on both sides of community entrances or as monumentation within a central island. Final locations shall be approved by the WVRC.

In all other areas, as permitted by the ULDC and as approved by the West Villages Review Committee.

### Gateway Monument Sign

Gateway Monument signs are intended to serve as an iconic entry feature to the West Villages. Gateway Monument signs may include major tenant names for the project.

**Area:** Maximum of 120 square feet per sign face.

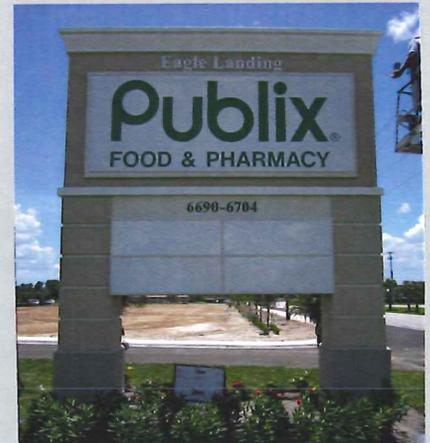


Figure 12.1

Pylon Sign



Figure 12.2

Monument Sign Residential



Figure 12.3

Monument Sign Non Residential



Figure 12.4

Gateway Monument Sign



Figure 12.5

### Entry Features



Figure 12.6

### Wayfinding Signs



Figure 12.7

### Directional Signs



Figure 12.8

### Light Pole Banners

## Other Signage Standards

### a. Entrance Features

1. All entrance features shall correspond with the theming of the development, as approved by the WVRC.
2. Select colors that are compatible with exterior colors, as approved by the WVRC. Keep color scheme simple and avoid combinations of primary or Day-Glow colors.

### b. Wayfinding Signs

1. Wayfinding signs are intended to orient the traveling public to destinations, neighborhoods and other places of interest.
2. Wayfinding signs shall be consistent with the overall development theme. These signs shall be a maximum of forty (40) square feet in copy/graphic area, and a maximum of ten (10) feet in height.

### c. Directional Signs

1. Directional signs, symbols or devices relating to traffic, parking, public services, facilities, or warnings on private property include, but are not limited to, "entrance", "exit", "slow", "no trespassing", "restrooms", and "telephones".
2. These signs shall be a maximum of twelve (12) square feet in copy/graphic area, and a maximum of four (4) feet in height.

### d. Light Pole Banners

1. Light pole banners may be installed in parking lot areas, along entrances to, and roadways within Mixed-Use and Mixed-Use Residential Neighborhoods.
2. These banners shall be exempt from regulation of quantity, location, and design.

The internal banner designs may be changed without review and approval of the City of North Port.

### Changeable copy/electronic signs

Changeable copy/ electronic signs shall be permitted as part of a pylon and/or monument sign, as determined by the WVRC.

One electronic sign utilizing 100% of the permitted sign area shall be permitted on the south side of the intersection of Tamiami Trail and West Villages Parkway.

The changeable copy portion of all other monument signs shall be included in total sign area calculations and the electronic portion shall not measure greater than 60% of the permitted sign area.

Signage shall be reviewed and approved by the WVRC.

### Building Signs

The following parameters apply to building signs. Local authority review, approval and permitting is also required.

All storefront signage and graphics are to be reviewed and approved by the WVRC. All sign packages shall include locations, sizes, colors, style of lettering, materials, types of illumination, installation details, and logo design.

1. No roof-mounted signs are permitted.
2. "Box" or "can" type signs are not permitted.
3. Any sign, notice, or other graphic display, particularly self-illuminated signs, located in the interior of the premises and easily legible from the exterior, except those which are required by applicable laws, codes, ordinances, or regulations, are permitted, upon approval of the WVRC.
4. All attachment devices, wiring, clips, transformers, lamps, tubes, bulbs, lighting sources, manufacturer's labels or plates, and other mechanisms required for signage shall be concealed from public view.
5. Letters may be interior-illuminated with lamps contained entirely within the depth of the letter.

6. Maximum of four (4) wall signs for that portion of the building that is leased/owned by a tenant/occupant, which is visible and/or accessible to the public. Maximum cumulative sign area shall be calculated as follows: ten percent (10%) of the surface area of the exterior wall included in an occupant's individually leased or owned premises, up to a maximum sign area of one hundred (100) square feet.
7. The following types of storefront signs will be permitted:
  8. Individual dimensional work or metal back-lit ("halo effect") letter. These letters must be a minimum of 6" in height.
  9. Internally-illuminated channel letters with opaque metal sides and translucent plastic faces. Letters must be at least 6" high; or Exposed neon tubes forming letters and/or logo; may be used in a decorative, as well as informative, manner.
  10. No exposed neon cross-over, raceways, ballast boxes, or transformers will be permitted for wall mounted signs. Raceways needed to support otherwise floating letters (not mounted to a wall) shall be painted to match the adjacent building. All penetrations of the building structure required for sign installation shall be sealed in a watertight condition and shall be patched to match adjacent finish.
  11. Sign company names or stamps must be concealed.
  12. The following types of signs and sign components and devices shall not be permitted unless otherwise approved by the WVRC:
    - Boxed or cabinet type, except where totally recessed;
    - Cloth, paper, cardboard and similar stickers or decals around or on the exterior surfaces of the storefront;
    - Exterior changeable letters or signs;
    - Signs painted directly on the storefront sign band;

## Signage Design and Installation Standards

### Blanks

Sign blanks must be 0.80-gauge aluminum; thicker, 0.125-gauge aluminum should be used for signs prone to vandalism, such as the 'no motorized vehicle' signs. Blanks must be covered with reflective sheeting of street transportation quality vinyl. There must be two, pre-drilled, 3/8-inch holes. The holes must be centered horizontally with the center of each hole being 1/2 inch from the top and bottom edges. Corners must be rounded with a 1-inch to 1 1/2 inch radius, depending on the size of the sign.

### Post Specification

Posts shall be constructed of 1 3/4" x 1 3/4", 12-gauge, square steel tubing with 7/16 inch, pre-punched knockouts on 1-inch centers. Post lengths must be 6'-0" feet. All steel posts shall be powder coated with color to match the design theme for the development. Prior to fabrication, color samples shall be submitted for approval by the WVRC.

### Sleeve and Anchor Specification

Sleeves and anchors shall be used in locations where it is possible for a vehicle to come into contact with the signpost, such as adjacent to a street. The use of the sleeve and anchor promotes easy breaking away of the sign post in the event of

a collision and increases the ease at which the sign can be replaced.

### Sign Mounting Hardware

8-inch, vandal resistant, steel drive rivets.

### Installation

Signposts adjacent to streets are to be installed according to the City of North Port Standard Details, and the MUTCD with style and finish as outlined in the Pattern Book and VDPP for the proposed development.

In non-roadside locations the signpost can be mounted directly into concrete. The finished height of the post should be 5'-6". Various combinations of signs can be mounted on a single post to address management needs of the particular area.

### Lettering

Font must be consistent (or similar styles)

Point size should be relative to the size of the sign.

### Colors

Background must be the same color, consistent with the overall theme of West Villages, with reflective lettering and symbols. For regulatory signs such as the 'no motorized vehicle' sign, shall be as required by the City.

## Building Criteria

### 1. General

Architectural design of all buildings shall be designed to be compatible with one another. Actual color and material shall be approved by the WVRC.

### 2. Sustainable Construction

All building construction will apply elements of the Leadership in Energy and Environmental (LEED) Green Building Rating System or Florida Green Building Coalition (FGBC) Florida Green Development, Commercial, High Rise Residential and Residential Development Standards, without the requirement to obtain certification from LEED or FGBC. At the time of application for development, a narrative shall be submitted to address what sustainable elements are used, and how they have been, or will be implemented.

### 3. Exterior Design Elements

The style of the development is intended to reflect a variety of architectural forms exemplified by the original architectural styles such as, but not limited to: Neo Classical, Old World European, Florida Coastal, Craftsman, Colonial, Federal, West Indies, and Mission, or other forms, as approved by the WVRC.

### 4. Exterior Materials

All materials shall be compatible with the overall scope of the West Villages. All materials and colors shall be submitted for review and approval by the WVRC. Acceptable materials include, but are not limited to:

- Asphalt Shingles (Dimensional)
- Hard Coat Section Stucco
- Clay Roof Tiles
- ArcusStone
- Concrete Roof Tiles
- Finish Concrete
- Cast Stone
- Marble
- Precast Stone
- Brick (possibly painted)
- Metal roof
- Siding
- Stone Veneer
- Natural stone
- Decorative shutters
- Aluminum awnings
- Cementitious siding

## 5. Exterior Requirements:

The following exterior standards are intended to ensure continuity of style and quality of appearance. This may be accomplished through articulated entrance(s), variegated roof lines, sloping roof planes, ordered variety of window shapes and sizes, and vertically and horizontally varied building masses.

No single architectural style has been established for the West Villages.

Materials should be selected which provide an appearance of weight, mass and permanence.

Wood, tile, and metal may be used for accent, embellishment, or accessory detailing only. These materials should generally not exceed 15% coverage on structures. (Windows and door frames, metal roofs and roll up doors are excluded from the coverage limitations).

Entries may be articulated. The use of overhangs, covered entries and courtyard entries is encouraged to create interest, provide weather shelter and foster an inviting pedestrian scale.

Color should be integral to the materials; where painted or applied finishes occur, use complementing hues.

Awnings are permitted, but should be used as accent, accessory elements only.

Surface printed signage is permitted as well as cast lighting onto awning surface for graphic illumination and awning accent.

Varied and pitched roof – Roofing finishes, dimensional shape and color shall be submitted for review and approval by the WVRC. Raised seam metal, dimensional architectural asphalt shingle and concrete tiles are encouraged as approved surfaces.

Retaining walls where required, should be carefully integrated into the building form or resolved into landscape materials.

## 6. Building Height

- Maximum building heights are established in the Development Standards of each VDPP.

## 7. Solar Collectors

Solar collectors may be permitted at locations approved by the WVRC.

## 8. Exterior Planting and Furniture

Open space areas are to be landscaped in accordance with the landscape theme established by this VDPB, as may be amended from time to time, and may be further defined during the VDPP review process. Modifications for individual Purchaser/Lessee identity, may be considered, and approved by the WVRC.

Purchaser/Lessee shall be permitted to place flower pots, window boxes, planters, and furniture within their entrance areas, subject to approval of the WVRC.

All furniture and bicycle racks should be durable and intended for exterior use.

Photograph or cut sheet on all furnishings will be submitted for review and approval by the WVRC, prior to installation or placement.



## Landscaping and Irrigation Standards

The following landscape standards are intended to provide guidance for future development within the West Villages, supporting open space areas that are functional, adaptable, and ecologically sound. These standards are intended to apply to all areas within the West Villages and may be further defined during the VDPP review process.

Landscape areas are four (4) separate zones; landscaping against major roads and parkways, against abutting properties, interior landscaping, and building perimeter landscaping.

All plant material used for landscaping shall meet or exceed the criteria established by the current edition of the Florida Grades and Standards for Nursery Plants.

Given the variability of the plant nursery market, canopy trees shall have a minimum 3-inch caliper and shall be a Florida #1 or better.

Hedges shall be a minimum of 24" tall, planted 3' on center at time of installation.

All other shrubs shall be a minimum of 12-18" in height with spacing noted on landscape plans.

All landscaping material and placement is subject to review and approval of the WVRC.

The WVRC<sup>2</sup> shall have authority to review and approve designs consistent with the Southwest Florida Water Management District (SWFWMD) Design Standards.

## Open Space

In each VDPP, minimum open space criteria shall be established for each land use area.

Open space areas may include, but should not be limited to the following:

- Buffers
- Landscaped areas in off-street parking areas
- Dry detention areas

Existing or proposed bodies of water, including stormwater management areas, consistent with the ULDC

- Active and passive recreation areas, such as playgrounds, golf courses, multimodal trails, and other similar open spaces
- Interior landscaped areas within commercial and Mixed-Use areas
- Building perimeter landscaping
- Pedestrian oriented hardscape areas such as plazas and outdoor dining spaces, when pervious materials

<sup>2</sup> Granting authority to the WVRC for review and approval of landscape plans does not eliminate the City's review authority to evaluate code-minimum landscape plans to confirm consistency with this proposed VDPB, the respective VDPP, and the ULDC.



### Village Perimeter Walls

Village perimeter walls are permitted within any commonly-owned open space tract or right-of-way within the Village. The village perimeter walls provide identity and definition to different uses and spaces that they separate throughout the Village. In addition, these walls provide separation, safety and tranquility for various uses and outdoor spaces in and out of the Village. Appropriate locations for such walls are around the Village edges, within the Village greenbelt, along neighborhood boundaries, along the Town Center boundary, and around any use within a Town Center. Village perimeter walls may be permitted up to ten (10) feet in height, exclusive of any berm. Village perimeter walls shall be constructed primarily as masonry walls, and may be accented with wood or wrought iron, upon approval of the WVRC. Chain link fencing may only be allowed if treated with vinyl cladding and landscaped with a continuous hedge at the base.

### Tree Canopy Development Standards

The City of North Port's ULDC requires a minimum of thirty-five percent (35%) canopy coverage for all individual lots, parcels and/or large developments. The West Villages is a large development, and therefore, the canopy coverage requirement shall be calculated on a village by village basis. The following standards shall apply to all development in West Villages.

#### General tree canopy development standards

In addition to trees that are preserved during development, all canopy trees that are planted shall count toward the minimum 35% canopy coverage requirement:

#### Residential street trees

One canopy tree shall be planted within twenty-five (25) feet of the right-of-way of each local street within a residential development for every fifty (50) linear feet, or substantial fraction thereof, or right-of-way when proposed lots have a minimum of one hundred (100) feet of frontage or greater. When proposed lots have less than one hundred (100) feet of frontage, street trees shall be limited to one (1) canopy tree per frontage. None of these required trees shall be planted within a public or private utility easement.

For proposed lots with less than sixty (60) feet of frontage, smaller maturing canopy trees or understory trees may be used.

The trees shall be spaced no closer together than twenty-five (25) feet, unless a decorative grouping or alternative method is chosen by the developer. Existing native trees should be used to fulfill these requirements wherever they meet the spacing and size requirements and are adequately protected during construction.

In order to ensure street trees are adequately protected and preserved, the developer shall provide a 15-foot landscape easement along all street frontages, on each Final Plat for residential development. The easement shall be to the benefit of West Villages Improvement District and/or their assignee(s), and the City of North Port. The easement shall protect the required street tree that is planted on individual residential lots, as detailed above. Tree maintenance and pruning shall be the responsibility of the property owner. Request for tree removal and replacement shall be in accordance with the ULDC, in effect at the time of request.

All canopy trees planted within the parking field, buffers, and building perimeter landscaping shall count toward replacement tree mitigation.

Neighborhoods in Village E that are developed exclusively for residential use will provide a minimum of 35% canopy coverage at maturity. Compliance will be demonstrated at time of Site and Development Plan submittal.

### **Town Center Canopy Trees**

To further the urban design character of the proposed Town Center and the desire to concentrate uses within a walkable, retail and entertainment district, canopy trees may be clustered based upon specific site designs to provide appropriate function and character to the space, as determined by the WVRC. Canopy trees will be utilized to provide natural shading within pedestrian environments and configured to allow sight lines to building facades and signage elements for the purposes of wayfinding. It is critical that trees placed within an urban environment have proper form at the time of planting.

Development in Town Center shall be required to provide a minimum of twenty percent (20%) canopy coverage at maturity. With the exception of Village E residential neighborhoods, this standard is not to be applied to individual parcels, but to all development areas within West Villages Town Center.

### Landscape Requirements for Major Roads and Parkways (As shown on the Index Map)

Street trees, shrubs, and sod within the right of way will be provided and installed by the developer and/or West Villages Improvement District.

All landscape requirements between the right-of-way and proposed development will be provided and installed by the developer, unless otherwise specified.

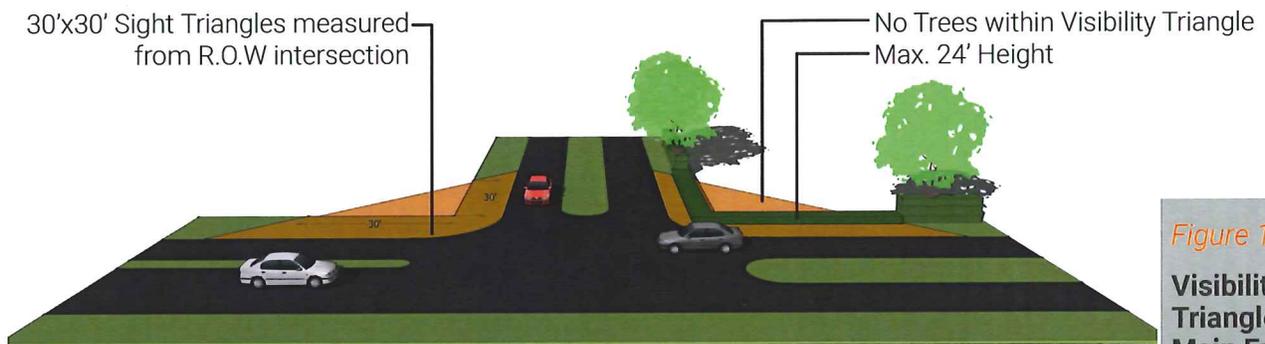
Berms may be used as landscape treatment. No building structures, except walls, freestanding signs, or fencing shall be constructed on the berm.

- a. There will be a landscaped frontage yard no less than 15' wide on the Purchaser/Lessee property between the right-of-way line and the paved ground surface area. It will be continuous along the entire right-of-way containing trees, shrubs, ground cover and turf grass (except for pedestrian and vehicular access locations).

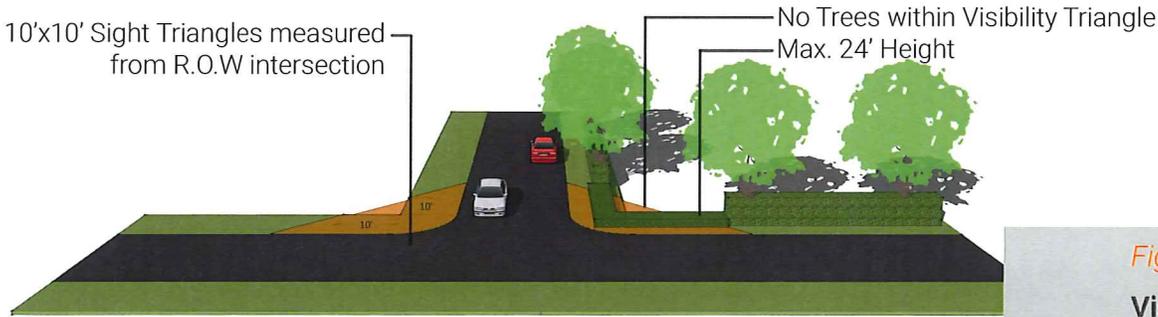


- b. The frontage yard shall include two (2) canopy trees and three (3) ornamental trees planted every 100 feet, or fraction of frontage yard. No canopy tree shall be planted closer than five (5) feet to the Purchaser/Lessee curb, or closer than seven (7) feet to a sidewalk.
- c. The opaque screen will be entirely of living landscaped material which will be a minimum of 24" in height at the time of building occupancy and must be continuous along the entire frontage yard. All shrub beds will be mulched.

- d. The entire frontage yard shall be irrigated. It is recommended that the main supply line be held tight to the inside Purchaser/ Lessee curb (as opposed to the right-of-way line) and to minimize soil disturbance in the natural vegetation areas.
  - e. Perimeter/Roadway buffers may be independent of the project boundary, and may include additional open space, lakes, and conservation/preservation areas, as long as the intent of the buffer is achieved with existing vegetation, and/or proposed plant material.
  - f. Perimeter/Roadway buffers may include a combination of berm, wall, fence, and plant material.
  - g. When the vehicular entrance/exit intersects a right-of-way or internal drive, all landscaping within the triangular areas described below shall allow unobstructed cross-visibility between 2' to 6' above finished grade. No trees shall be permitted within the visibility triangle. Only turf or ground cover will be permitted closer than 3' to the entrance/exit paved surface.
8. The triangular areas are:
- At the main entrance drives to large commercial tracts, from intersection of the dedicated right-of-way line of major parkways with an access right-of-way line to 2 points each 30' along those lines and connecting those 2 points defines the second cross visibility triangle. See Figure 12.11.
  - At interior roads, from intersection of the right-of-way line with entrance/exit road edge line to 2 points each 10' along those lines and connecting those 2 points defines the first cross visibility triangle. See Figure 12.12.



**Figure 12.11**  
**Visibility Triangles at Main Entry Drives**



*Figure 12.12*  
**Visibility Triangles at Interior Roads**

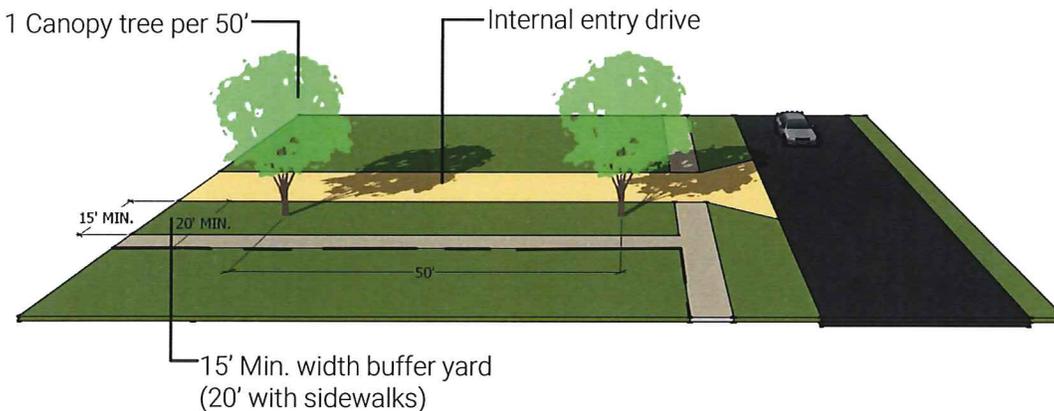
**Abutting Property Requirements**

Abutting property requirements will apply between the Purchaser/Lessee and (1) common properties / drives, (2) other adjacent Purchaser/Lessee, and (3) private West Villages space. For all three conditions, there shall be a continuous landscape buffer yard the entire length of the common property between the property line and the Purchaser/Lessee’s paved ground surface area. Entrance/exit or cross access paving width plus three-feet each side will be deducted when computing the buffer yard length requirements. The buffer yard minimum width shall be as detailed below. There shall be no ground surface left uncovered (shrubs, ground cover or turf) and it shall be irrigated. All plant materials shall be from Appendix A of this VDPB.

Site improvements shall include coordination and construction of a concrete sidewalk to the adjacent parcel(s) property line to promote positive pedestrian circulation throughout development parcels (See Adopted Index Map).

**a. Against common internal street/entry drives:**

For all buildings that are not built to within 10 feet of internal roadways, there shall be one canopy tree planted for every 50 linear feet or fraction thereof in a 15’ minimum (20’ at required sidewalks) width buffer yard. Tree species shall be per Appendix A of this VDPB. For buildings that are built to within 10 feet of internal roadways, no landscaping shall be required, but provided for in the Right-of-Way for those internal streets/entry drives, and consistent with roadway standards contained in this VDPB.



*Figure 12.13*  
**Abutting Property Requirements Against common internal street/entry drives**

**1st Purchaser:**

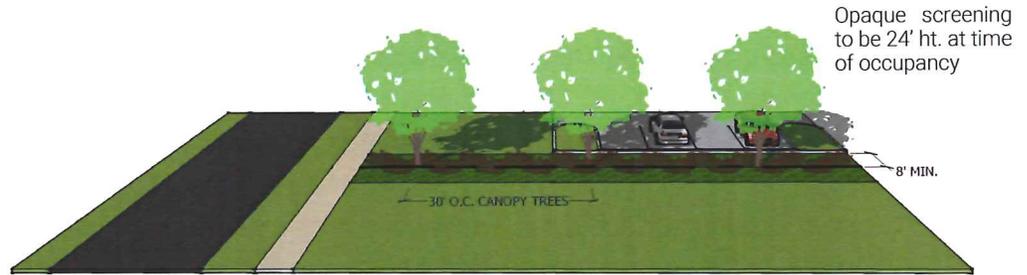
- Install 1 canopy tree every 30 linear ft. or fraction thereof.
- Maintain 8' min. buffer yard.
- Install Opaque screen of living landscape material, ground cover, and sod

**2nd Purchaser:**

- Is not required to install a landscape buffer yard against the common property line

Figure 12.14

**Abutting Property Requirements Against Adjacent Purchaser/Lessee Properties**



**b. Against other adjacent Purchaser/Lessee properties:**

The buffer yard shall be a minimum of 8' wide. The Purchaser/Lessee who causes initial construction (1st Purchaser/Lessee) shall install canopy trees for every 30 linear feet or fraction thereof, an opaque screen of living landscape material (minimum 24" in height), groundcover and sod.

The second Purchaser/Lessee will not be required to provide or install a landscape buffer yard against the common property line.

Where Purchasers/Lessees abut one another in a common access drive, the required landscape buffer may be deleted.

Internal street/drive buffers shall not be required for Mixed-Use Street A (see Mixed-Use Street A Cross Section).

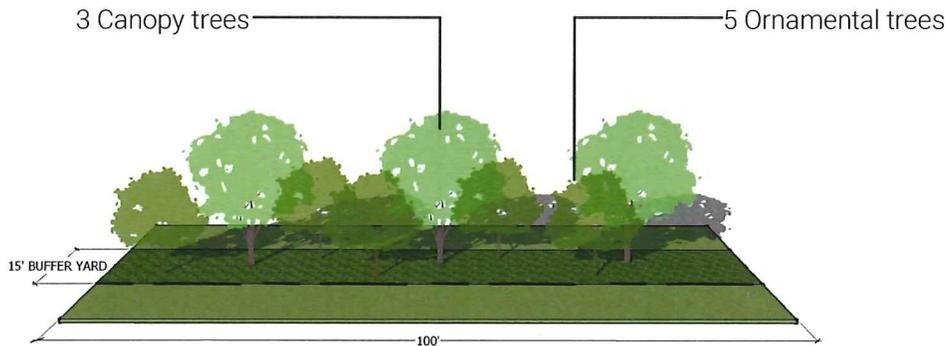


Figure 12.15

**Abutting  
Property  
Against Private  
West Villages  
Property**

**a. Against private West Villages property:**

The buffer yard shall be a minimum of 15' wide. Within the buffer yard, three (3) canopy trees, five (5) ornamental trees and 33 shrubs shall be planted for every 100 linear feet or fraction thereof of the buffer yard, and shall comprise a minimum 5 to 7 feet of the plantable area. Groundcover and sod shall fill the remainder of the planting buffer. New trees shall be planted to match the species of the West Villages existing trees. Purchaser/Lessee shall plant and irrigate to the right of way line or back of sidewalk within private West Villages property as required.

Where adjacent property is to be developed as a continuation of commercial development and/or parking area, the buffer yard shall not be required.

Where future development of adjacent property is residential or unknown, the buffer yard shall be required.

If the adjacent property has provided a buffer which meets or exceeds the buffer width and plant quantities identified, the buffer yard and plantings shall not be required.

### **Interior Landscape Requirements for non-residential and mixed-use development**

Interior landscape areas are in the parking field between the building perimeter curb and the outer parking lot edge. Interior landscape area requirements are in addition to roadway or abutting property requirements, unless otherwise approved. Interior landscape areas shall conform to the requirements for vehicular use areas in the City of North Port's ULDC and any additional requirements stated herein.

Palm trees may be utilized, in lieu of canopy trees in interior landscape areas along the exterior of parking lots. If single trunk palms (free standing application) are used, they shall serve as a 1:1 replacement for canopy trees. If single trunk palms (cluster application) are used, a cluster of 3 palms shall count as one canopy tree.

Sidewalks should provide direct and clear connection from all public rights-of-way to the façade of all development/buildings, as determined by the WVRC. At a minimum, one coordinated pedestrian sidewalk, interior to the development, connecting across and through parking lots to serve buildings and/or sites should be provided. Parking spaces shall not disrupt sidewalk connections to building entries.

Alternative designs may be reviewed and approved by the WVRC.

All parking spaces shall provide either wheel stops or curbing, unless otherwise approved by the WVRC.

Where interior landscape areas abut the roadway, the roadway buffer may serve as the perimeter parking lot buffer. However, where the vehicle use area does not abut a roadway, the perimeter landscaping requirements shall be a minimum width of eight (8) feet containing two (2) canopy trees per one hundred (100) linear feet or substantial fraction thereof, and either shrubs, hedges, berms or fences or any combination thereof.

All landscaped buffer areas and sidewalks adjacent to off-street parking areas shall be protected from encroachment of vehicles with curbs and/or wheel stops, unless otherwise approved by the WVRC. Wheel stops and/or curbs shall have a minimum height of six (6) inches above finished grade of the parking area. Wheel stops shall be properly anchored. Where wheel stops are located two (2) feet from the front of a parking space, that two (2) feet is not required to be paved, but shall require the use of pervious pavers or other similar treatment, as approved by the WVRC. However, the area between the wheel stop and the landscaped area shall receive appropriate landscape treatment, including planting of grass or ground cover. Where wheel stops are not used, all landscape material, excluding sod/ground cover, shall be installed a minimum of two feet from the face of curb. All parking spaces, except parallel parking spaces, that abut landscaped areas or sidewalks within a parking lot shall have wheel stops or curb to prevent obstruction within the landscaped areas, unless otherwise approved by the WVRC.

Parallel parking spaces located adjacent to landscape areas shall include a minimum two foot (2') paved strip adjacent to the curb to protect landscaping from damage, and provide safe and efficient pedestrian circulation.

### **Building Perimeter Landscape Requirements**

Foundation plantings shall provide visual breaks along monotonous building façades, provide direction to and enhance entrances, and be used or installed to screen mechanical equipment that is attached to or adjacent to the building.

The Purchaser/Lessee is encouraged to employ creative design and include such amenities as earth mounding, water features, landscape lighting, various sidewalk materials and patterns, boulders, sculpture, etc. The design intent should be toward an entranceway that enhances and complements the building's architecture.

#### **Minimum Planting Requirements (Commercial/Mixed-Use):**

**Calculation:** One foundation plant or shrub shall be required for every 3 linear feet, and one ornamental tree or palm shall be required for every 30 linear feet of publicly visible façade (i.e. excluding alley/internal drive or loading area).

**Location:** Foundation landscaped areas shall be a minimum of 2 feet in width. Clustered planting locations shall occupy a minimum of 20%, and be located within 30 feet of publicly visible façade. A concrete or stone/gravel rodent strip a minimum of 12" wide shall be provided between the face of the building and foundation landscape areas.

For buildings that are built to within 10 feet of internal roadways, no landscaping shall be required, but provided for in the Right-of-Way for those internal streets/entry drives, and consistent with roadway standards in the VDPP.

#### **Minimum Planting Requirements (Single-use Residential within Town Center/Mixed-Use Areas):**

**Calculation:** One foundation plant or shrub shall be required for every 3 linear feet and one ornamental tree or palm shall be required for every 30 linear feet of all buildings.

**Location (Single-use Residential within Mixed-Use Areas):**

Foundation landscaped areas shall be located no less than 3 feet and no more than 5 feet from the face of the building.

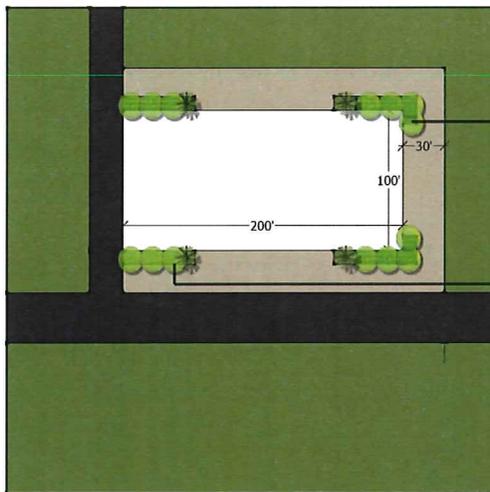
If plantings are clustered, they shall occupy a minimum of 50% of building facade, and be located within 15 feet of the building.

For buildings that are built to within 10 feet of internal roadways, no landscaping shall be required, but provided for in the Right-of-Way for those internal streets/entry drives, and consistent with roadway standards in the VDPP.

The transitional space between public and private space offers an opportunity to establish a “sense of arrival” through the use of specialty paving, plazas, decorative lighting, and site furnishings. The use of such elements shall be encouraged throughout the Village E Mixed-Use Areas.

Figure 12.16

**Building Perimeter Landscape Requirements Mixed Use/Commercial**



Building Perimeter Requirements-Commercial/Mixed Use 5' min. width for foundation landscaping areas

Clustered Planting locations shall occupy 30% of publicly visible building facade and be located within 30' of facade

3' min. and 15' max. with for foundation landscaping areas

Clustered Planting locations shall occupy 50% of publicly visible building facade and be located within 15' of facade

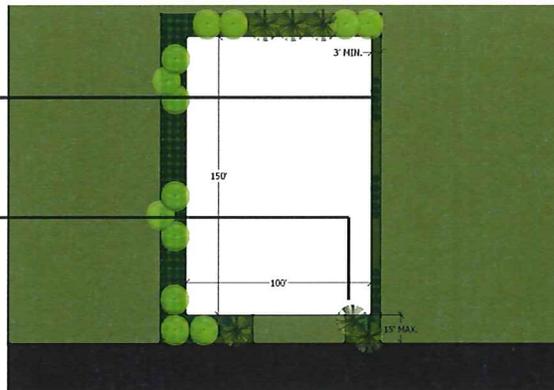


Figure 12.17

**Building Perimeter Landscape Requirements Single-use Residential**

## Screening

All structures and facilities for trash, storage, loading and outdoor equipment must be screened so as not to be visible from the street and pedestrian circulation areas. At a minimum, screening shall be either an opaque screen of evergreen shrubs and/or evergreen trees or by solid walls at least 6 feet in height. Shrubs must be at least 4 feet in height with a minimum 2 foot spread and trees at least 6 feet in height with a 4 foot minimum spread at the time of building occupancy. Shrubs shall be spaced no further apart than 3 feet and trees spaced at the most appropriate spacing for the species; but no further than 12 feet on center. Walls should be designed to express the appropriate West Villages theme and be coordinated with the building architecture.

To the extent possible, expanses of solid building wall area without architectural detail should be minimized. Where solid building wall area are without architectural detail, they may not exceed 50' in length without being covered by landscape treatment unless the building wall faces an alley, the rear of another building or a loading/service area that is not facing a street or driveway, and is not visible to the public.

Fifty percent of a blank wall that exceeds 50 linear feet, and is visible to the public, shall either be covered or accentuated with planting areas near the foundation of the building that contain large maturing trees and shrubs/vines. Planting requirements shall be calculated as follows:

For every 10 feet beyond 50 feet of blank wall, one maturing canopy tree or palm cluster, and 3 shrubs or vines shall be provided, and such materials should be planted within 30' of the façade. As an alternative, the required materials may be incorporated into the required foundation landscaping planting areas in those areas where the blank wall exists.

## Lighting Standards

The implementation of thoughtful lighting standards is an important part of the sustainability, livability, and sense of place within the West Villages. Ensuring lighting standards remain consistent with modern technology is essential to maintaining a sense of safety and security within the West Villages, while mitigating the excess illumination and habitat disruption. These standards encourage a combination of sustainable bulbs and fixtures in order to prevent the negative effects of excessive lighting which may result in the visibility or negative impacts to the environment. Consistent with the practices of the International Dark Sky Association lighting in the West Villages should:

- Only be on when needed
- Only light the area that needs it
- Be no brighter than necessary
- Be fully shielded (pointing down)

### Protected area (Outdoor Lighting)

1. in residential areas
2. in an area that may develop with predominantly residential use,
3. in a mixed-use area that may develop into a predominantly residential use
4. in a public street right-of-way.

### Lighting sources

LEDs or low spectrum

Metal halide bulbs

High Pressure Sodium Vapor (HPSV)

Solar Lights

*Lighting with a lower color temperature than 3000 kelvins is encouraged*

### **Fixtures and shielding**

Appropriate source light shielding is encouraged in order to minimize glare, protect dark skies, while facilitating better vision at night. All fixtures shall be black unless otherwise approved by the WVRC, standard FPL poles and fixtures are permitted.

With these standards and practices in mind all developments shall submit lighting plans to be reviewed and approved by the WVRC at the site and development phase. Lighting standards shall be consistent with this document and further refined through the Village District Pattern Plan process.

Lighting Plans shall include:

1. Fixture location
2. Fixture detail
3. Fixture heights and mounts
4. Direct illumination plots

### **Measurable Spill Illumination Limits**

The total illumination caused by all of a property's outdoor lighting, including light sources, diffraction, and reflections from on-site objects, shall not illuminate another property in excess of the following limits:

- 0.2 foot-candles on property with outdoor lighting protected areas but not including the street rights-of-way.
- 1.5 foot-candles on street rights-of-way because such limited coincidental illumination of a street will not constitute a nuisance to people using the street.
- Minimum of 0.5 foot-candles on pedestrian sidewalks and paths.

### **Fixture Height**

Fixture height, measured from finished grade at ground level to the bottom of the light source, determines the vertical point from which the light projects downward. Regardless of the following height limits, prevention of spill illumination may require lower heights according to calculations and plots in the lighting plan.

- a. All light fixtures located within fifty (50) feet of any other property that has an outdoor lighting protected area shall not exceed twenty (20) feet in height.
- b. All other light fixtures elsewhere shall not exceed thirty-five (35) feet in height, unless approved by the WVRC.





# Appendix A: Preferred Plant List

## STREET TREES

| Common Name | Scientific Name    |
|-------------|--------------------|
| Live Oak    | Quercus virginiana |

## CANOPY TREES

| Common Name   | Scientific Name         |
|---------------|-------------------------|
| Magnolia      | Magnolia grandiflora    |
| Cypress       | Taxodium spp.           |
| Laurel Oak    | Quercus laurifolia      |
| Live Oak      | Quercus virginiana      |
| Holly         | Ilex spp.               |
| Red Maple     | Acer rubrum             |
| Sweet Gum     | Liquidambar styraciflua |
| Slash Pine    | Pinus elliotii          |
| Elm           | Ulmus spp.              |
| Sycamore      | Platanus occidentalis   |
| Black Olive   | Bucida buceras          |
| Red Cedar     | Juniperus virginiana    |
| Wild Tamarind | Lysiloma latisiliquum   |
| Gumbo Limbo   | Bursera simaruba        |

## SINGLE TRUNK PALMS (cluster application) Minimum cluster of 3 palms shall count as one canopy tree

| Common Name      | Scientific Name       |
|------------------|-----------------------|
| Cabbage Palm     | Sabal palmetto        |
| Pindo Palm       | Butia capitata        |
| Queen Palm       | Syagrus romanzoffiana |
| Washington Palm  | Washingtonia robusta  |
| Foxtail Palm     | Wodyetia bifurcata    |
| Chinese Fan Palm | Livistona chinensis   |

**Note: Some palms may appear in both the cluster and the free-standing single trunk applications.**

**SINGLE TRUNK PALMS (free standing application) Each palm shall count as one canopy tree**

| <b>Common Name</b>  | <b>Scientific Name</b>        |
|---------------------|-------------------------------|
| Sylvester Date Palm | Phoenix sylvestris            |
| Royal Palm          | Roystonea elata               |
| Travelers Palm      | Ravenala madagascariensis     |
| Medjool Date Palm   | Phoenix dactylifera 'Medjool' |
| Foxtail Palm        | Wodyetia bifurcata            |
| Bismarck Palm       | Bismarckia nobilis            |
| Chinese Fan Palm    | Livistona chinensis           |

**Note: Some palms may appear in both the cluster and the free-standing single trunk applications.**

**MULTI-STEM PALMS**

| <b>Common Name</b> | <b>Scientific Name</b>     |
|--------------------|----------------------------|
| Alexandra Palm     | Archontophoenix alexandrae |
| Areca Palm         | Dyopsis lutescens          |
| Macarthur Palm     | Ptychosperma macarthurii   |
| Paurotis Palm      | Acoelorrhaphe wrightii     |
| Senegal Date Palm  | Phoenix reclinata          |
| Christmas Palm     | Adonidia merrillii         |
| Pygmy Date Palm    | Phoenix roebelenii         |

**ORNAMENTAL TREES/ACCENTS**

| <b>Common Name</b>               | <b>Scientific Name</b>       |
|----------------------------------|------------------------------|
| Crape Myrtle                     | Lagerstroemia indica         |
| Indian Hawthorn standard         | Rhaphiolepis Majestic Beauty |
| Jacaranda                        | Jacaranda mimosifolia        |
| Oleander standard                | Nerium oleander              |
| Royal Poinciana                  | Delonix regia                |
| Pink Trumpet Tree                | Tabebuia heterophylla        |
| Golden Trumpet Tree              | Tabebuia chrysotricha        |
| Tree Hibiscus                    | Hibiscus rosa-sinensis       |
| Weeping Bottlebrush (or regular) | Callistemon viminalis        |
| Southern Red Cedar               | Juniperus silicicola         |
| Necklace Pod                     | Sophora tomentosa            |
| Dahoon Holly                     | Ilex cassine                 |
| Loquat                           | Eriobotrya japonica          |
| Tree Ligustrum                   | Ligustrum japonica           |

|                              |                             |
|------------------------------|-----------------------------|
| Weeping Podocarpus           | Podocarpus gracilior        |
| White Bird of Paradise       | Strelitzia nicolai          |
| Jatropha                     | Jatropha integerrima        |
| Wax Myrtle                   | Myrica cerifera             |
| Golden Shower                | Cassia fistula              |
| King Sago Palm               | Cycas revoluta              |
| Tibouchina/Purple Glory Tree | Tibouchina granulosa        |
| European Fan Palm            | Chamaerops humilis          |
| Seagrape                     | Coccoloba uvifera           |
| Silver Buttonwood            | Conocarpus erectus sericeus |

**SHRUBS****Common Name****Scientific Name**

|                                  |  |
|----------------------------------|--|
| Florida Privet                   | Forestiera segregata                             |
| Pittosporum                      | Pittosporum tobira                               |
| Seagrape                         | Coccoloba uvifera                                |
| Holly                            | Ilex spp.  |
| Podocarpus                       | Podocarpus spp.                                  |
| Silver Buttonwood                | Conocarpus erectus sericeus                      |
| Viburnum                         | Viburnum spp.                                    |
| Hibiscus                         | Hibiscus rosa-sinensis                           |
| Ixora                            | Ixora spp.                                       |
| Oleander (dwarf or regular size) | Nerium oleander                                  |
| Simpson Stopper                  | Myrcianthes fragrans                             |
| Anise                            | Illicium spp.                                    |
| Bush Allamanda                   | Allamanda neriifolia                             |
| Ligustrum                        | Ligustrum japonicum                              |
| Beauty Berry                     | Callicarpa americana                             |
| Variegated Ginger                | Alpinia zerumbet 'variegata'                     |
| Orange Jasmine/Chalcas           | Murraya paniculata                               |
| Peace Lily                       | Spathiphyllum spp.                               |
| Bamboo                           | Bambusa spp. (non-invasive – clumping type only) |
| Japanese Boxwood                 | Buxus microphylla japonica                       |
| Cocoplum                         | Chrysobalanus spp.                               |
| Firebush                         | Hamelia patens                                   |
| Thryallis                        | Galphimia glauca                                 |
| Dwarf Schefflera                 | Heptapleurum arboricola                          |
| Pampas Grass                     | Cortaderia selloana                              |
| Philodendron                     | Philodendron spp.                                |

Plumbago  
Saw Palmetto  
Yucca  
Canna Lily  
Downy Jasmine  
Shining Jasmine  
Golden Dew Drop  
Orange Bird of Paradise  
Cardboard  
Gardenia  
Giant Crinum Lily

Plumbago auriculata  
Serenoa repens  
Yucca spp.  
Cannas spp.  
Jasminum multiflorum  
Jasminum nitidum  
Duranta erecta  
Strelitzia Reginae  
Zamia furfuracea  
Gardenia spp.  
Crinum spp.

## GROUNDCOVER

### Common Name

Blue Daze  
Coontie  
Natal Plum  
Lily Turf  
Perennial Peanut  
Lily of the Nile  
Daylily  
Juniper  
Lantana  
Mexican Heather  
Society Garlic  
Flax Lily  
African Iris  
Holly Fern  
Swordfern  
Minima Jasmine

### Scientific Name

Evolvulus glomeratus  
Zamia floridana  
Carissa spp.  
Liriope spp.  
Arachis glabrata  
Agapanthus africanus  
Hemerocallis spp.  
Juniperus spp.  
Lantana spp.\*  
Cuphea hyssopifolia  
Tulbaghia violacea  
Dianella spp.  
Dietes vegeta  
Cyrtomium falcatum  
Nephrolepis spp.\*  
Trachelospermum aslaticum 'minima' (green or variegated)

**VINES**

**Common Name**

**Scientific Name**

|                     |   |
|---------------------|---|
| Cross Vine          | Bignonia capriolata                               |
| Bougainvillea       | Bougainvillea spp.                                |
| Coral Honeysuckle   | Lonicera sempervivens                             |
| Confederate Jasmine | Trachelospermum jasminoides (green or variegated) |
| Flame Vine          | Pyrostegia venusta                                |
| Passion Vine        | Passiflora incarnate                              |
| Creeping Fig        | Ficus pumila (repens)                             |
| Carolina Jessamine  | Gelsemium sempervivens                            |

**GRASSES**

**Common Name**

**Scientific Name**

|                         |                          |
|-------------------------|--------------------------|
| Cordgrass               | Spartina bakeri          |
| Fakahatchee Grass       | Tripsacum dactyloides    |
| Dwarf Fakahatchee Grass | Tripsacum floridana      |
| Fountain Grass          | Pennisetum setaceum      |
| Muhly Grass             | Muhlenbergia capillaries |

\*Excluding species listed on the Florida Exotic Pest Plant Council's (FLEPPC) list of invasive species.

•Low wind tolerant trees and palms.

-Plant species identifiable are not all inclusive and may be amended; Plant types and sizes may be substituted, depending on plant availability, as approved by the WVRC.

# Definitions

**Civic buildings/Center.** A building or complex of buildings that houses government offices and services and that may include cultural, recreational, athletic, convention, and entertainment facilities.

**Greenbelts.** As outlined in the Village Zoning Ordinance No. 2000-31, greenbelts shall be required at the perimeter of each village. These greenbelts may function as an environmental corridor(s) incorporating passive recreation uses such as hiking and surface water management systems. Greenbelts may also include Eco-Trails as discussed in this document.

**Neighborhood.** Distinct residential districts within Villages, comprised generally of no more than 500 AGA (adjusted gross acres). Two or more Neighborhoods are needed to form a Village.

**Neighborhood Center.** Neighborhood center areas are intended to serve residential neighborhoods as community centers providing a concentration of activities that serve the immediate area. These neighborhood centers may be community centers for residential neighborhoods, passive or active parks, neighborhood greens, or civic nodes. These spaces shall be shown at the subdivision plan stage.

**Open Space.** Includes stormwater management areas, golf courses, floodplains, greenbelts, upland habitat areas, and vehicular/utility corridors and takes the form of squares, greens, and parks whose frequent use is encouraged through access, placement, and design.

**Open Space Systems.** This is defined as the network of open space that covers the West Villages.

**Platting.** To divide or sub-divide lands into lots, blocks, parcels, tracts, sites, or other divisions however designated.

**Solar Collectors.** A device or combination of devices, structure, or part of a device or structure that transforms direct solar energy into thermal, chemical, or electrical energy.

**Town Center.** Town Center areas are intended to offer residents of The West Villages access to a broad range of retail/commercial, office, civic, and residential uses. Provision of Town Centers allows for uses that support a more regional market base than Village Centers, thus needs of Village E neighborhoods are met through this development.

**Village.** A collection of Neighborhoods, Civic Spaces, Village Parks, and a Village or Town Center as well as other ancillary uses. The West Villages is made up of Villages A-L.

**Village Center.** Village Center areas consist of a variety of residential, commercial/ service, and retail uses that support the neighborhoods that comprise the village. Consistent with the Village Index Map, Village E is dedicated to Town Center development, which will offer residents of The West Villages access to a broad range of retail/commercial, office, civic, and residential uses filling the traditional role of Village Center.